

CITY OF BEAVERTON
DESIGN EXCEPTION REQUEST

For City Engineering Office use only

Control No: _____

Project Name:		FC/ROW/SD/CIP #:
Project Location:		

PROJECT DATA

Functional Classification:		<input type="checkbox"/> Arterial <input type="checkbox"/> Collector <input type="checkbox"/> Neighborhood Route <input type="checkbox"/> Local <input type="checkbox"/> Other			
Current ADT (Year):				Design ADT (Year):	
Posted Speed:	mph	Design Speed:	mph	Anticipated Approval Date:	
Funding:		<input type="checkbox"/> Private <input type="checkbox"/> City <input type="checkbox"/> State <input type="checkbox"/> Federal			
Current Estimate:				Additional Cost to Meet Standard:	
NHS:	<input type="checkbox"/>	Top 10% SPIS Site:	Yes	<input type="checkbox"/>	
Non NHS:	<input type="checkbox"/>		No	<input type="checkbox"/>	

Design Exceptions for which version: 2007 <input type="checkbox"/> 2019 <input type="checkbox"/>		
<input type="checkbox"/> Design Speed	<input type="checkbox"/> Lane Width	<input type="checkbox"/> Bridge Rail
<input type="checkbox"/> Horizontal Alignment	<input type="checkbox"/> Parking Width	<input type="checkbox"/> Pavement Design Life
<input type="checkbox"/> Vertical Alignment	<input type="checkbox"/> Bike Lane/Multi-Use Path Width	<input type="checkbox"/> Vertical Clearance
<input type="checkbox"/> Intersection Sight Distance	<input type="checkbox"/> Median Width	<input type="checkbox"/> Super elevation
<input type="checkbox"/> Stopping Sight Distance	<input type="checkbox"/> Length of Cul-de-sac	<input type="checkbox"/> Super elevation Runoff
<input type="checkbox"/> Intersection / Driveway Spacing	<input type="checkbox"/> Shy Distance	<input type="checkbox"/> Clear Zone
<input type="checkbox"/> Pavement Cross Slope	<input type="checkbox"/> ADA Standards	<input type="checkbox"/> (Other)
<input type="checkbox"/> Grade	<input type="checkbox"/> Bridge Width	Sidewalk Width ¹

¹ Needs a Sidewalk Modification from Planning.

Description of Project:

Description of Exception:

Location of Design Feature:

Reasons For Not Attaining Standard: (Such As Cost/ Benefit, Crash History, Environmental, Etc.)

Effect on Other Standards:

Compatibility with Adjacent Sections and Context:

Mitigation for Exception Included In Design:

Supporting Documentation (Include the appropriate Plan Section, Cross Section, Alignments Sheets & Plan Details):

**CITY OF BEAVERTON
DESIGN EXCEPTION REQUEST**

Signatures

Prepared By: _____ **Date:** _____
(Engineer of Record)

Print Name:	_____	Phone:	_____
Company Name:	_____		
Company Address:	_____		
City:	_____	ST:	_____
Email Address:	_____		

Concurred By: _____ **Date:** _____
(Site Development Division Manager, Public Works Director,
OPS Program Manager, etc.)

(Print Name)

Concurred By: _____ **Date:** _____
(City Traffic Engineer)


(Print Name)

Approved By: _____ **Date:** _____
(City Engineer)

(Print Name)

PREPARED BY:

**ENGINEER OF RECORD
PROFESSIONAL
ENGINEER STAMP**



EXPIRES: 12/31/25

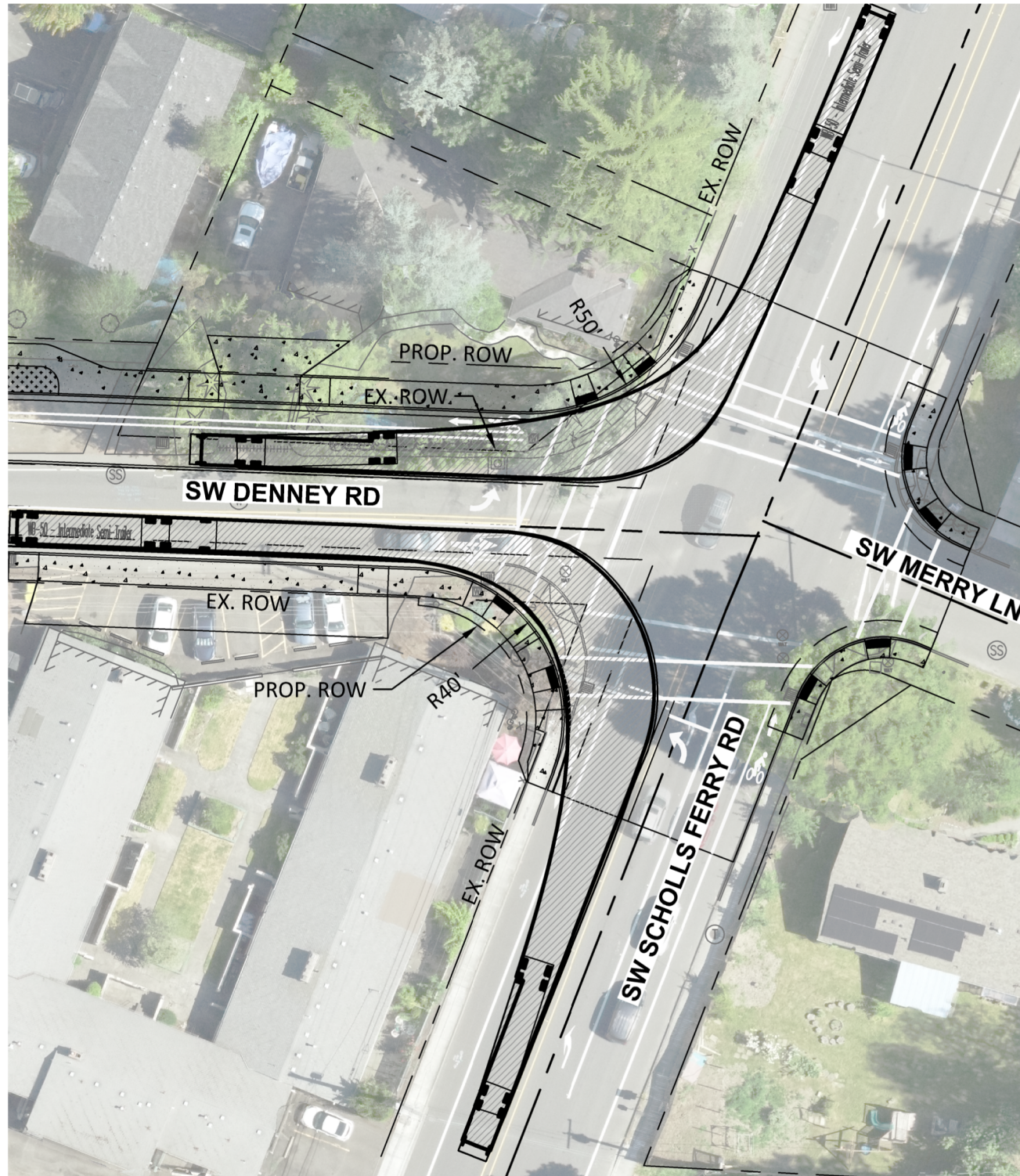
Curb Return Radii Design Exception Request – Additional Information

Reason For Not Attaining Standard: The primary reason for not meeting the standard is the impact larger curb radii would have on the adjacent properties. Per section 210.19 of the Engineering Design Manual, the intersection of SW Denney Rd and SW Scholls Ferry Rd must accommodate a WB-65 Interstate Semitrailer Design Vehicle because SW Scholls Ferry Rd is designated as a Truck Route in the Comprehensive Plan Transportation Element. To accommodate a WB-65, the northwest corner of SW Denney Rd and SW Scholls Ferry Rd would have a curb radius of 75' and the southwest corner of SW Denney Rd and SW Scholls Ferry Rd would have a curb radius of 50'. These larger curb radii would require additional right of way acquisition and would impact existing structures, likely requiring the purchase of two residential properties. Larger curb radii would also increase the pedestrian crossing distance making the intersection less safe for pedestrians. See the attached Truck Turning Exhibit for details for a comparison of the proposed design to the footprint needed to meet the design requirement.

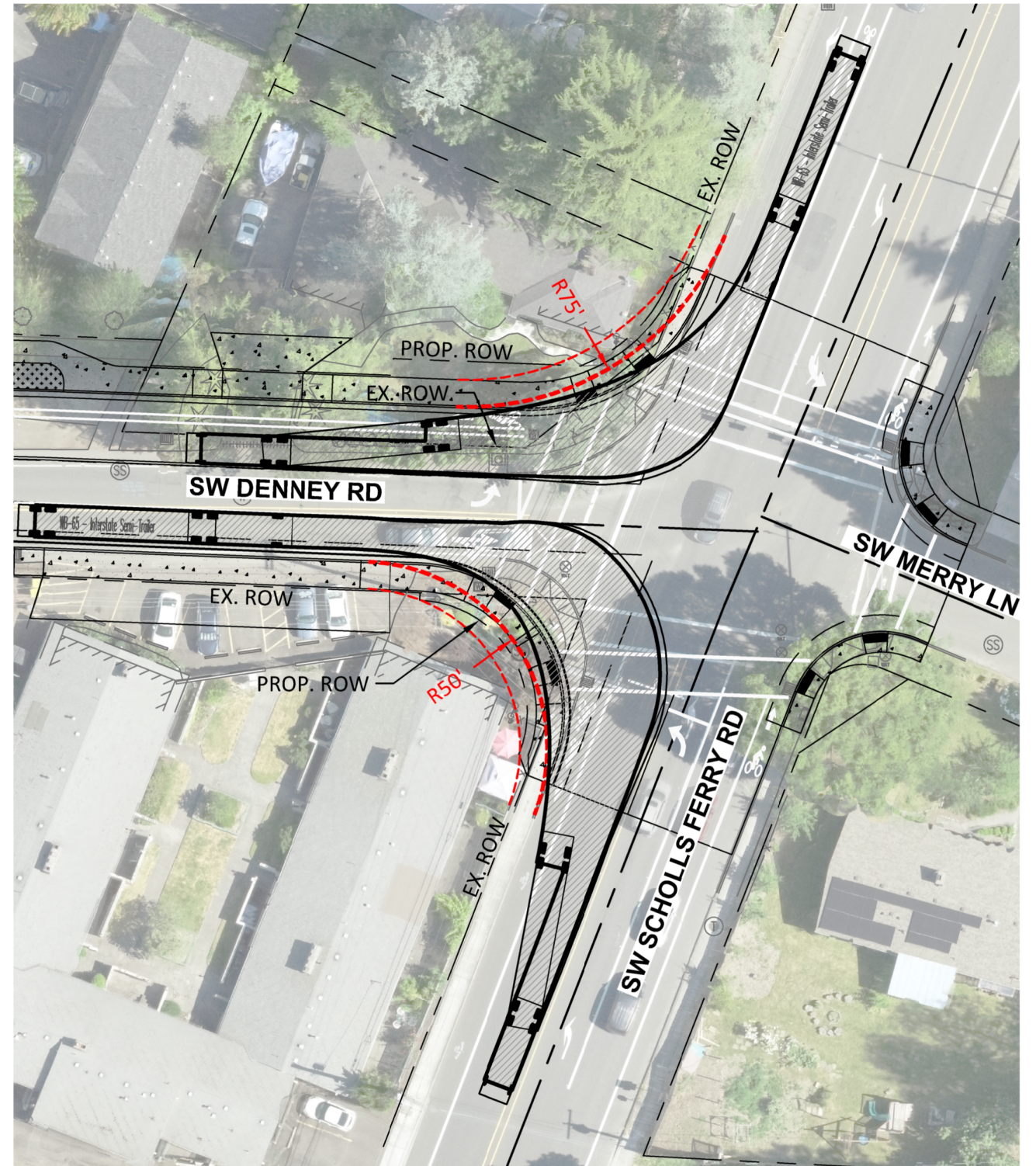
The NW and SW corners of SW Denney Rd and SW Scholls Ferry Rd currently have radii of 45' and 25' respectively. The proposed design utilizes a curb radius of 50' at the northwest corner of SW Denney Rd and SW Scholls Ferry Rd, and a 40' radius at the southwest corner to accommodate a right turning movement of a WB-50. This significantly improves vehicle maneuverability through the intersection while keeping impacts to adjacent private properties to a minimum.

A review of the five-year crash data shows there were 7 incidents at this intersection, 3 turning collisions, 3 rear-end collisions, and 1 pedestrian collision. There is no crash history for the intersection related to the existing curb return radii. As a result, the proposed design which increases the curb radii (while not meeting standard) will not have a negative impact to public safety. Likewise, reconstructing the curb ramps to current ADA standards will improve public safety for pedestrians.

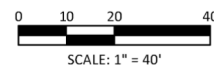
Compatibility with Adjacent Sections and Context: SW Scholls Ferry Rd falls under the jurisdiction of Washington County; the proposed intersection geometry at SW Denney Rd and SW Scholls Ferry Rd meets the Washington County requirements of 40' edge of pavement/curb minimum per section 320.060 of the Washington Country Road Standards. The proposed design also meets the AASHTO guidance of a 40' curb radius for intersections that accommodate frequent bus and truck use (section 9.6.1.4 of the 2018 publication of *A Policy on Geometric Design of Highways and Streets*).



DESIGN PROPOSED TO ACCOMMODATE A WB-50 VEHICLE



DESIGN REQUIRED TO ACCOMMODATE A WB-65 VEHICLE



DATE	NO.	DESCRIPTION
R E V I S I O N S		

DESIGNED:	HHPR
DRAWN:	HHPR
CHECKED:	BRA
DATE:	MARCH 2024


Harper Houf Peterson Righellis Inc.
ENGINEERS • PLANNERS
 LANDSCAPE ARCHITECTS • SURVEYORS
 3330 SW Jefferson Way, Suite 200, Beaverton, OR 97002
 phone: 503.358.1133 www.hhpr.com fax: 503.251.1171

TRUCK TURNING EXHIBIT SW DENNEY RD BEAVERTON, OREGON	SHEET NO. 1 OF 1 JOB NO. WAS-39
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**CITY OF BEAVERTON
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Current Estimate:		Additional Cost to Meet Standard:		
NHS:	<input type="checkbox"/>	Top 10% SPIS Site:	Yes	<input type="checkbox"/>
Non NHS:	<input type="checkbox"/>		No	<input type="checkbox"/>

Design Exceptions for which version: 2007 <input type="checkbox"/> 2019 <input type="checkbox"/>		
<input type="checkbox"/> Design Speed	<input type="checkbox"/> Lane Width	<input type="checkbox"/> Bridge Rail
<input type="checkbox"/> Horizontal Alignment	<input type="checkbox"/> Parking Width	<input type="checkbox"/> Pavement Design Life
<input type="checkbox"/> Vertical Alignment	<input type="checkbox"/> Bike Lane/Multi-Use Path Width	<input type="checkbox"/> Vertical Clearance
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<input type="checkbox"/> Pavement Cross Slope	<input type="checkbox"/> ADA Standards	<input type="checkbox"/> (Other)
<input type="checkbox"/> Grade	<input type="checkbox"/> Bridge Width	Sidewalk Width ¹

¹ Needs a Sidewalk Modification from Planning.

Description of Project:

Description of Exception:

Location of Design Feature:

Reasons For Not Attaining Standard: (Such As Cost/ Benefit, Crash History, Environmental, Etc.)

Effect on Other Standards:

Compatibility with Adjacent Sections and Context:

Mitigation for Exception Included In Design:

Supporting Documentation (Include the appropriate Plan Section, Cross Section, Alignments Sheets & Plan Details):

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Prepared By: _____ **Date:** _____
(Engineer of Record)

Print Name:		Phone:	
Company Name:			
Company Address:			
City:		ST:	Zip:
Email Address:			

Concurred By: _____ **Date:** _____
(Site Development Division Manager, Public Works Director,
OPS Program Manager, etc.)

(Print Name)

Concurred By: _____ **Date:** _____
(City Traffic Engineer)


(Print Name)

Approved By: _____ **Date:** _____
(City Engineer)

(Print Name)

PREPARED BY:

**ENGINEER OF RECORD
PROFESSIONAL
ENGINEER STAMP**



EXPIRES: 12/31/25

Horizontal Alignment Design Exception Request – Additional Information

Reason For Not Attaining Standard: The primary reason for this design exception request is that a smaller centerline radius allows for the protection of more significant trees and private improvements, which has been a priority for this project. Utilizing a minimum CL radius of 475' would increase the length of the transition, pushing it closer to the intersection of SW Scholls Ferry Rd reducing the available queuing length, while also pushing further west requiring the removal of several high priority trees.

To reduce impact on trees, private improvements, and encourage reduced vehicle speeds, the design team selected a minimum CL radius of 305 feet based on table 210.10 of the Engineering Design Manual. This corresponds to a design speed of 30 mph. While the design speed of SW Denney Rd is 35 mph, design criteria based on a lower speed is justified based on the nature of the roadway and the posted speed limit of 30 mph.

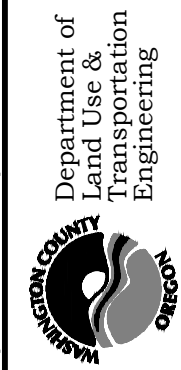
The CL transition between Sta. 28+63.16 and 30+85.34 has the greatest horizontal shift of the two CL transitions that utilize a radius of 305', at 27', but is near the signalized intersection of SW Denney Rd and SW Scholls Ferry Rd. Vehicle speeds are expected to be lower than the posted speed through these curves at vehicles approach or exit the intersection.

Mitigations for Exception Included in Design: This project has several proposed traffic calming measures to reduce the speeds along the corridor and support the use of a reduced centerline radius. Striped cross walks, rectangular rapid flashing beacons, and reduced lane widths will all reduce speeds along the corridor.



EXPIRES: 12/31/25

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LEGEND

- FULL DEPTH PAVEMENT SECTION
SEE TYPICAL SECTION FOR DETAILS
- 0 - 2" GRIND AND 2" INLAY
SEE TYPICAL SECTION FOR DETAILS

NO.	REVISION
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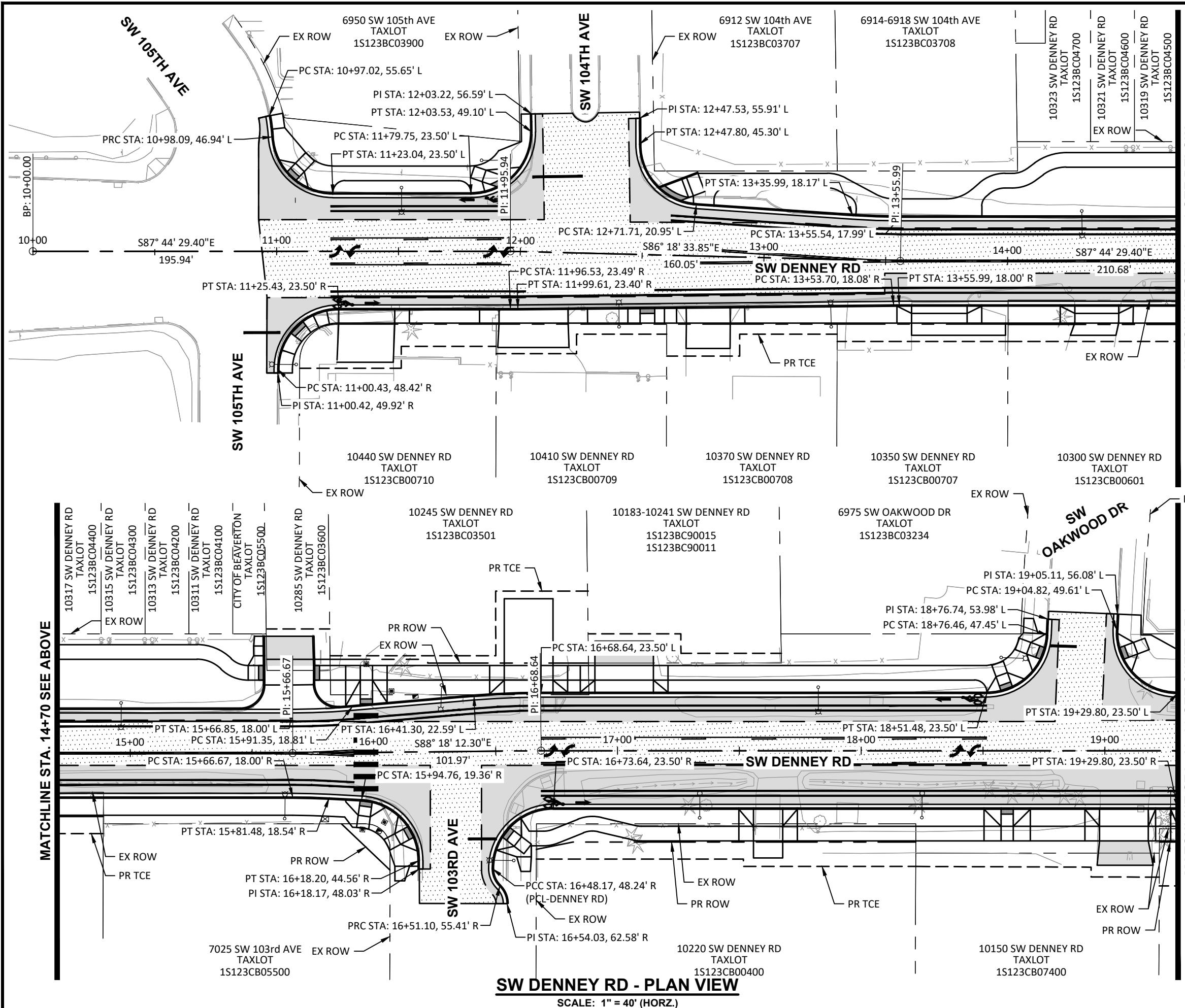
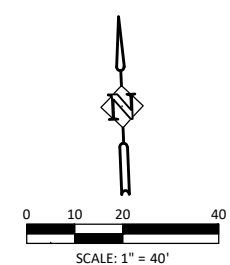
SW DENNEY ROAD
 SW 105TH AVE TO SW SCHOLLS FERRY ROAD
 WASHINGTON COUNTY

SW DENNEY RD STA POB TO 19+30

GEOMETRY AND PAVING PLANS

PROJECT NUMBER
100666

SHEET NO.
33 OF **84**
 SHEET TITLE
2F



SW DENNEY RD - PLAN VIEW
 SCALE: 1" = 40' (HORZ.)

MATCHLINE STA. 14+70 SEE BELOW

MATCHLINE STA. 19+30 SEE SHEET 2F-2

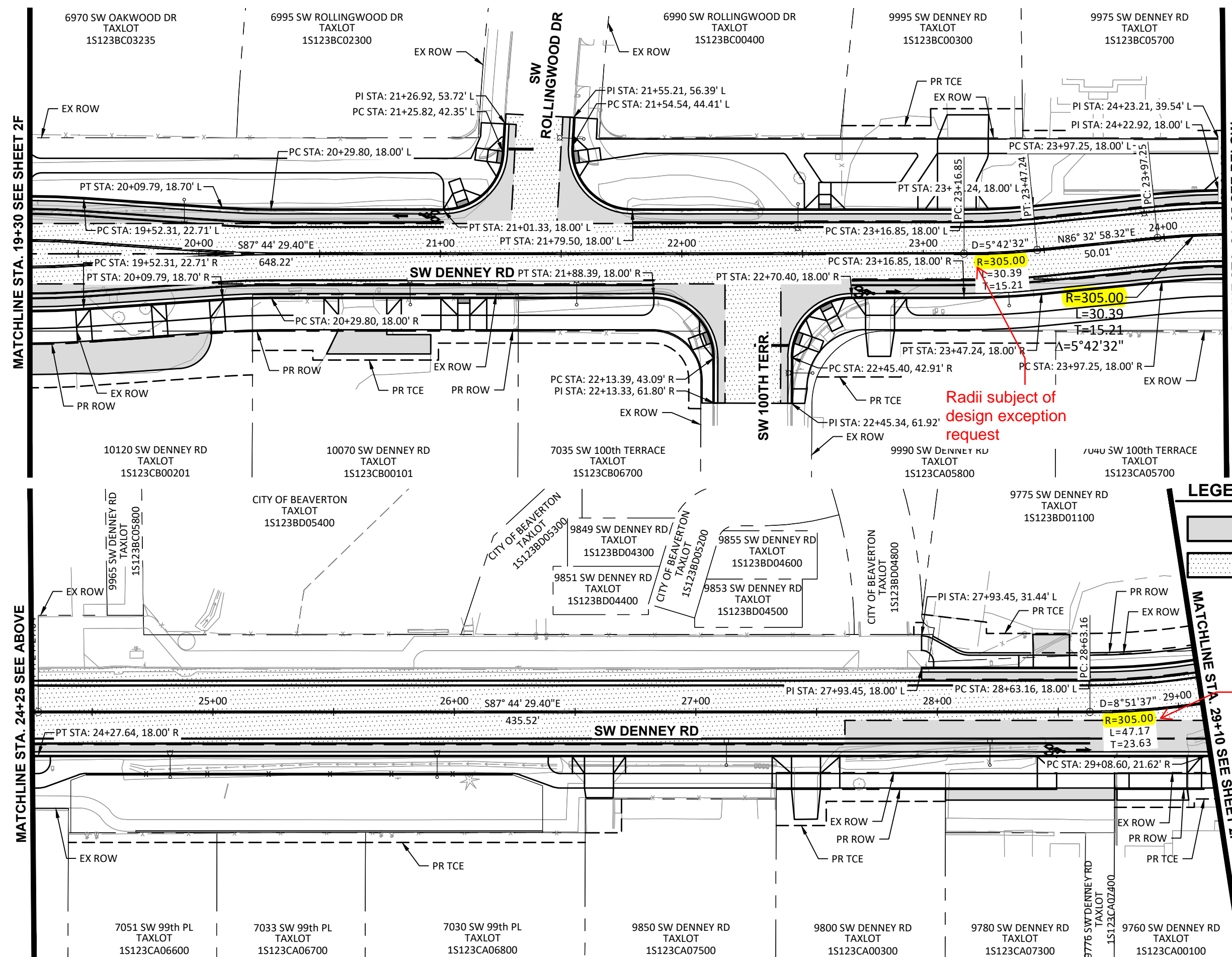
MATCHLINE STA. 14+70 SEE ABOVE



EXPIRES: 12/31/25

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Department of Land Use & Transportation Engineering



MATCHLINE STA. 19+30 SEE SHEET 2F

MATCHLINE STA. 24+25 SEE BELOW

MATCHLINE STA. 24+25 SEE ABOVE

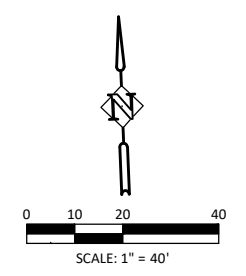
LEGEND

- FULL DEPTH PAVEMENT SECTION SEE TYPICAL SECTION FOR DETAILS
- 0 - 2" GRIND AND 2" INLAY SEE TYPICAL SECTION FOR DETAILS

NO.	REVISION
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MATCHLINE STA. 29+10 SEE SHEET 2F-3

Radius subject of design exception request



SW DENNEY RD - PLAN VIEW
 SCALE: 1" = 40' (HORZ.)

SW DENNEY ROAD
 SW 105TH AVE TO SW SCHOLLS FERRY ROAD
 WASHINGTON COUNTY

SW DENNEY RD STA 19+30 TO 29+10
GEOMETRY AND PAVING PLANS

PROJECT NUMBER
100666

SHEET NO.
34 OF 84

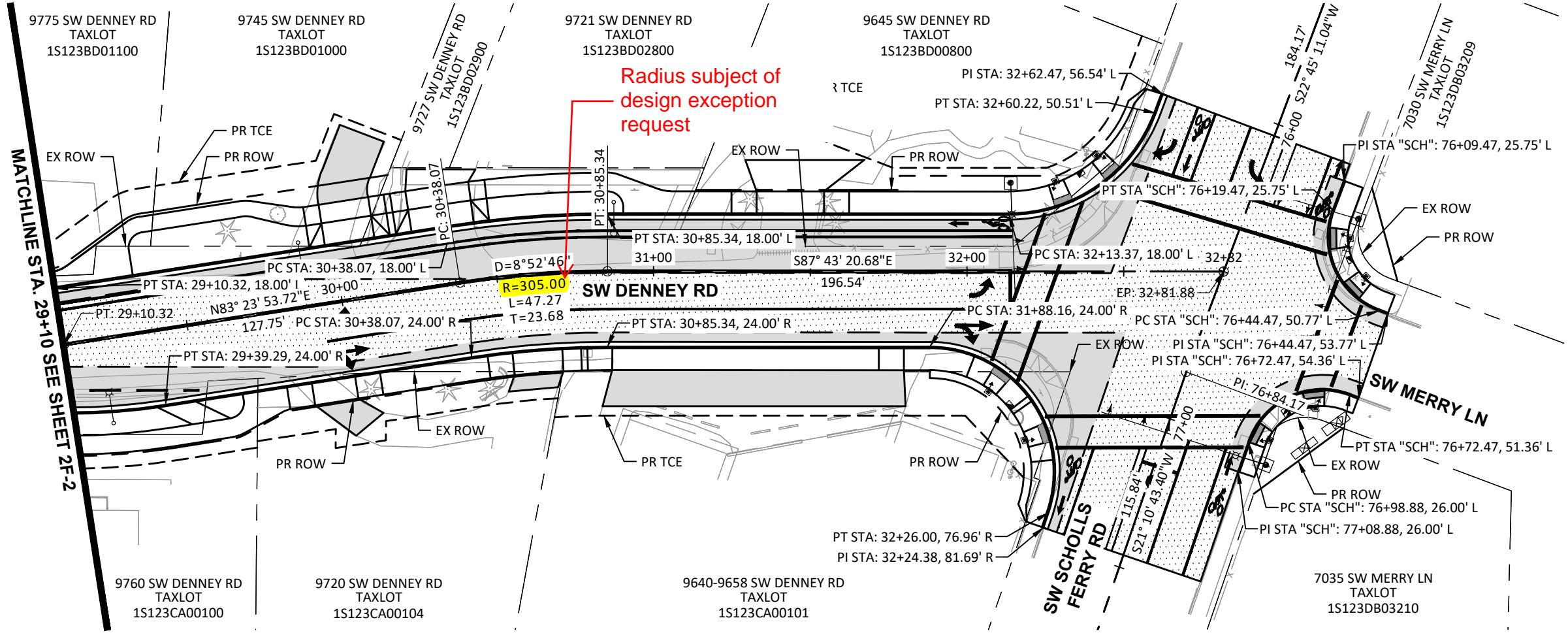
SHEET TITLE
2F-2



EXPIRES: 12/31/25

Harper
Houf Peterson
Righellis Inc.

ENGINEERS, PLANNERS
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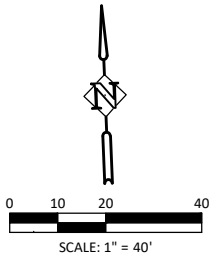


Radius subject of design exception request

SW DENNEY RD - PLAN VIEW
SCALE: 1" = 40' (HORZ.)

LEGEND

- FULL DEPTH PAVEMENT SECTION
SEE TYPICAL SECTION FOR DETAILS
- 0 - 2" GRIND AND 2" INLAY
SEE TYPICAL SECTION FOR DETAILS



NO.	REVISION
1	
2	
3	
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SW DENNEY ROAD
SW 105TH AVE TO SW SCHOLLS FERRY ROAD
WASHINGTON COUNTY

SW DENNEY RD STA 29+10 TO EOP
GEOMETRY AND PAVING PLANS

PROJECT NUMBER
100666

SHEET NO.
35 OF **84**

SHEET TITLE
2F-3

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PROJECT DATA

Functional Classification:	<input type="checkbox"/> Arterial <input type="checkbox"/> Collector <input type="checkbox"/> Neighborhood Route <input type="checkbox"/> Local <input type="checkbox"/> Other			
Current ADT (Year):		Design ADT (Year):		
Posted Speed:	mph	Design Speed:	mph	Anticipated Approval Date:
Funding:	<input type="checkbox"/> Private <input type="checkbox"/> City <input type="checkbox"/> State <input type="checkbox"/> Federal			
Current Estimate:		Additional Cost to Meet Standard:		
NHS:	<input type="checkbox"/>	Top 10% SPIS Site:	Yes	<input type="checkbox"/>
Non NHS:	<input type="checkbox"/>		No	<input type="checkbox"/>

Design Exceptions for which version: 2007 <input type="checkbox"/> 2019 <input type="checkbox"/>		
<input type="checkbox"/> Design Speed	<input type="checkbox"/> Lane Width	<input type="checkbox"/> Bridge Rail
<input type="checkbox"/> Horizontal Alignment	<input type="checkbox"/> Parking Width	<input type="checkbox"/> Pavement Design Life
<input type="checkbox"/> Vertical Alignment	<input type="checkbox"/> Bike Lane/Multi-Use Path Width	<input type="checkbox"/> Vertical Clearance
<input type="checkbox"/> Intersection Sight Distance	<input type="checkbox"/> Median Width	<input type="checkbox"/> Super elevation
<input type="checkbox"/> Stopping Sight Distance	<input type="checkbox"/> Length of Cul-de-sac	<input type="checkbox"/> Super elevation Runoff
<input type="checkbox"/> Intersection / Driveway Spacing	<input type="checkbox"/> Shy Distance	<input type="checkbox"/> Clear Zone
<input type="checkbox"/> Pavement Cross Slope	<input type="checkbox"/> ADA Standards	<input type="checkbox"/> (Other)
<input type="checkbox"/> Grade	<input type="checkbox"/> Bridge Width	Sidewalk Width ¹

¹ Needs a Sidewalk Modification from Planning.

Description of Project:

Description of Exception:

Location of Design Feature:

Reasons For Not Attaining Standard: (Such As Cost/ Benefit, Crash History, Environmental, Etc.)

Effect on Other Standards:

Compatibility with Adjacent Sections and Context:

Mitigation for Exception Included In Design:

Supporting Documentation (Include the appropriate Plan Section, Cross Section, Alignments Sheets & Plan Details):

**CITY OF BEAVERTON
DESIGN EXCEPTION REQUEST**

Signatures

Prepared By: _____ **Date:** _____
(Engineer of Record)

Print Name:		Phone:	
Company Name:			
Company Address:			
City:		ST:	Zip:
Email Address:			

Concurred By: _____ **Date:** _____
(Site Development Division Manager, Public Works Director,
OPS Program Manager, etc.)

(Print Name)

Concurred By: _____ **Date:** _____
(City Traffic Engineer)

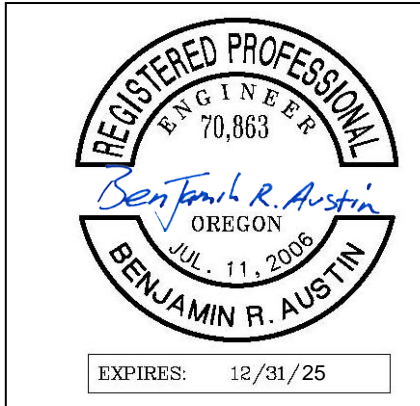
(Print Name)

Approved By: _____ **Date:** _____
(City Engineer)

(Print Name)

PREPARED BY:

**ENGINEER OF RECORD
PROFESSIONAL
ENGINEER STAMP**



Lane Width Design Exception Request – Additional Information

Reasons for Not Attaining Standard: The primary reasons for not meeting the 12' lane width requirements along this corridor is the limited and inconsistent existing right of way, as well as the roadway proximity to existing, significant trees. Providing increased protection to cyclists along the corridor was of high importance – the design team concluded that a reduced lane width in combination with the addition of a 2' bike lane buffer meets the needs of the corridor more effectively than the standard roadway section presented in Standard Detail 200-2. Reduced lane widths allow for all components of the proposed section in areas of limited right of way while limiting the impacts to existing trees and private improvements.

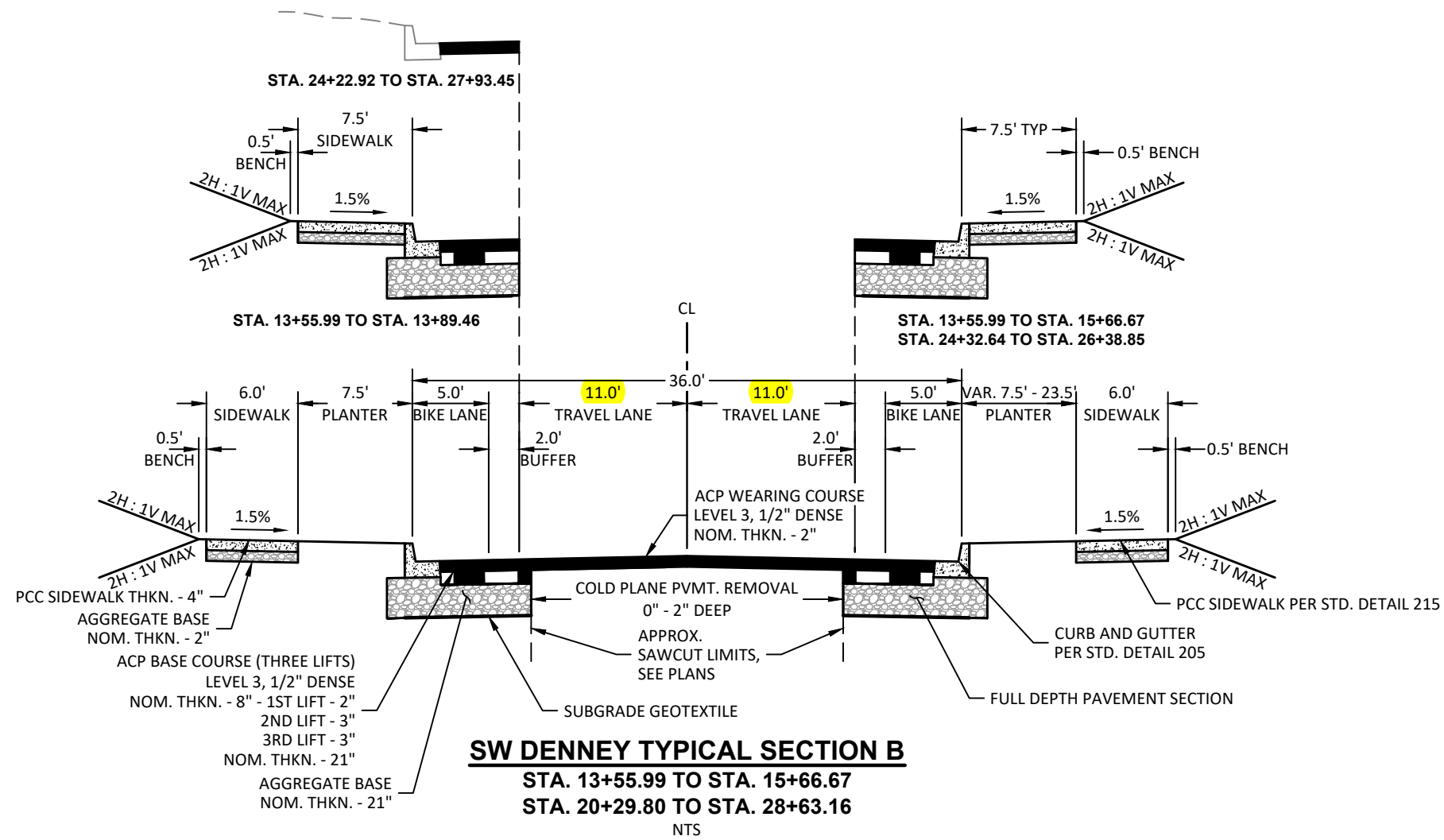
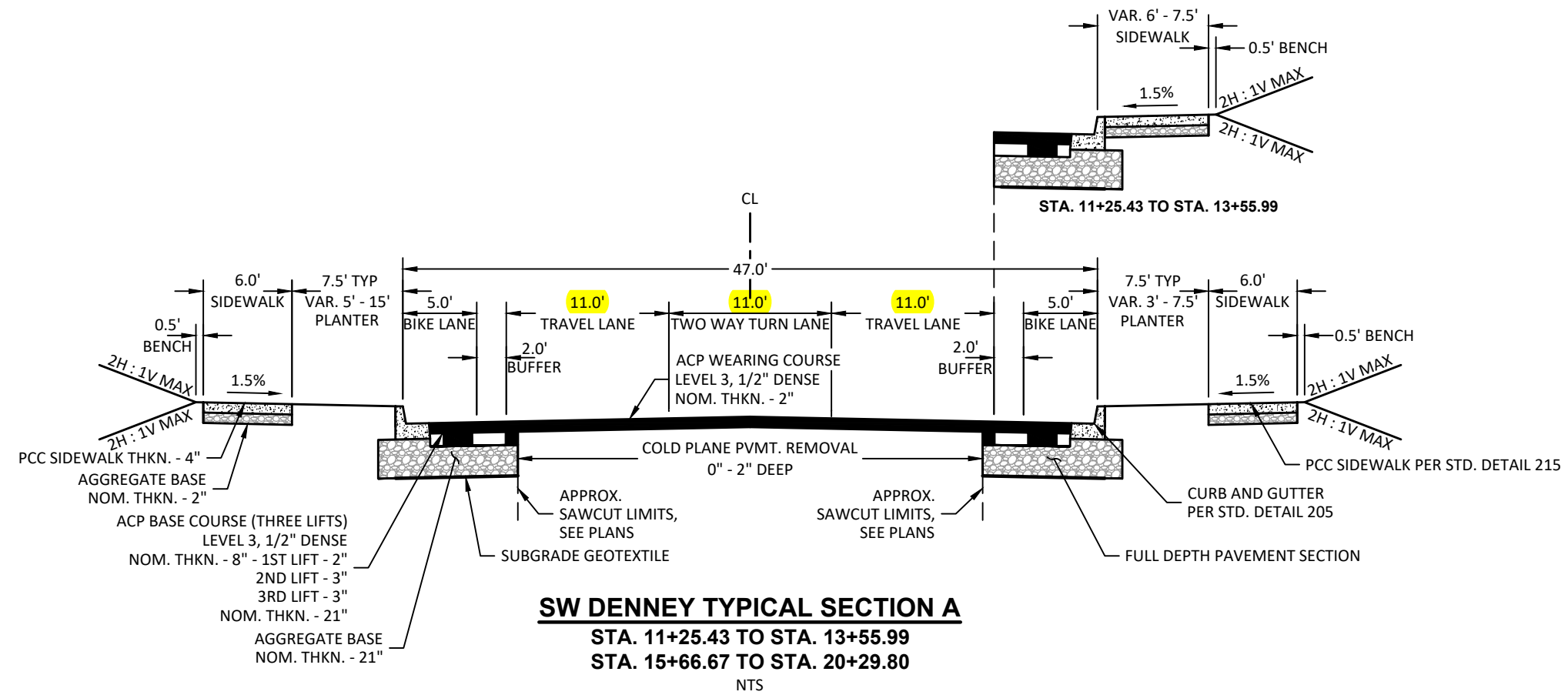
Mitigations for Exception Included in Design: Traffic calming measures including striped cross walks, RRFBs, and reduced centerline radii will all reduce speeds along the corridor and support the use of reduced lane widths. 2' buffers between the bike lane and travel lane are included to provide additional separation and protection to cyclists.



EXPIRES: 12/31/25

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Department of Land Use & Transportation Engineering



NO.	REVISION
1	
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SW DENNEY ROAD
 SW 105TH AVE TO SW SCHOLLS FERRY ROAD
 WASHINGTON COUNTY

TYPICAL SECTIONS

PROJECT NUMBER
100666

SHEET NO.
 4 OF 84

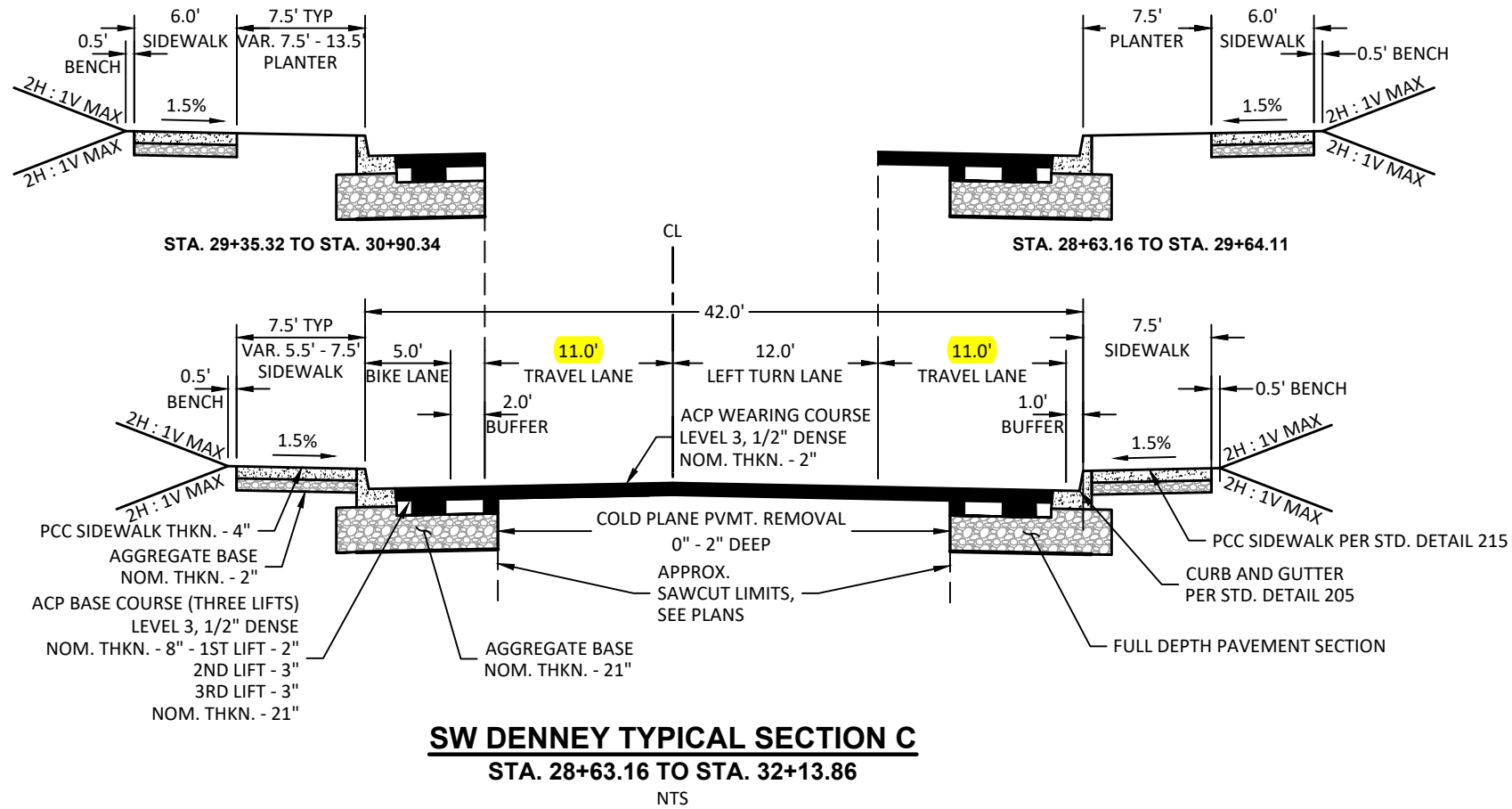
SHEET TITLE
2A



EXPIRES: 12/31/25

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Department of Land Use & Transportation Engineering



NO.	REVISION
1	
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SW DENNEY ROAD
 SW 105TH AVE TO SW SCHOLLS FERRY ROAD
 WASHINGTON COUNTY

TYPICAL SECTIONS

PROJECT NUMBER
100666

SHEET NO.
5 OF **84**

SHEET TITLE
2A-2



EXPIRES: 12/31/25

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Department of Land Use & Transportation Engineering
 WASHINGTON COUNTY

MATCHLINE STA. 14+70 SEE BELOW

LEGEND

- FULL DEPTH PAVEMENT SECTION
SEE TYPICAL SECTION FOR DETAILS
- 0 - 2" GRIND AND 2" INLAY
SEE TYPICAL SECTION FOR DETAILS

NO.	REVISION
1	
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SW DENNEY ROAD
 SW 105TH AVE TO SW SCHOLLS FERRY ROAD
 WASHINGTON COUNTY

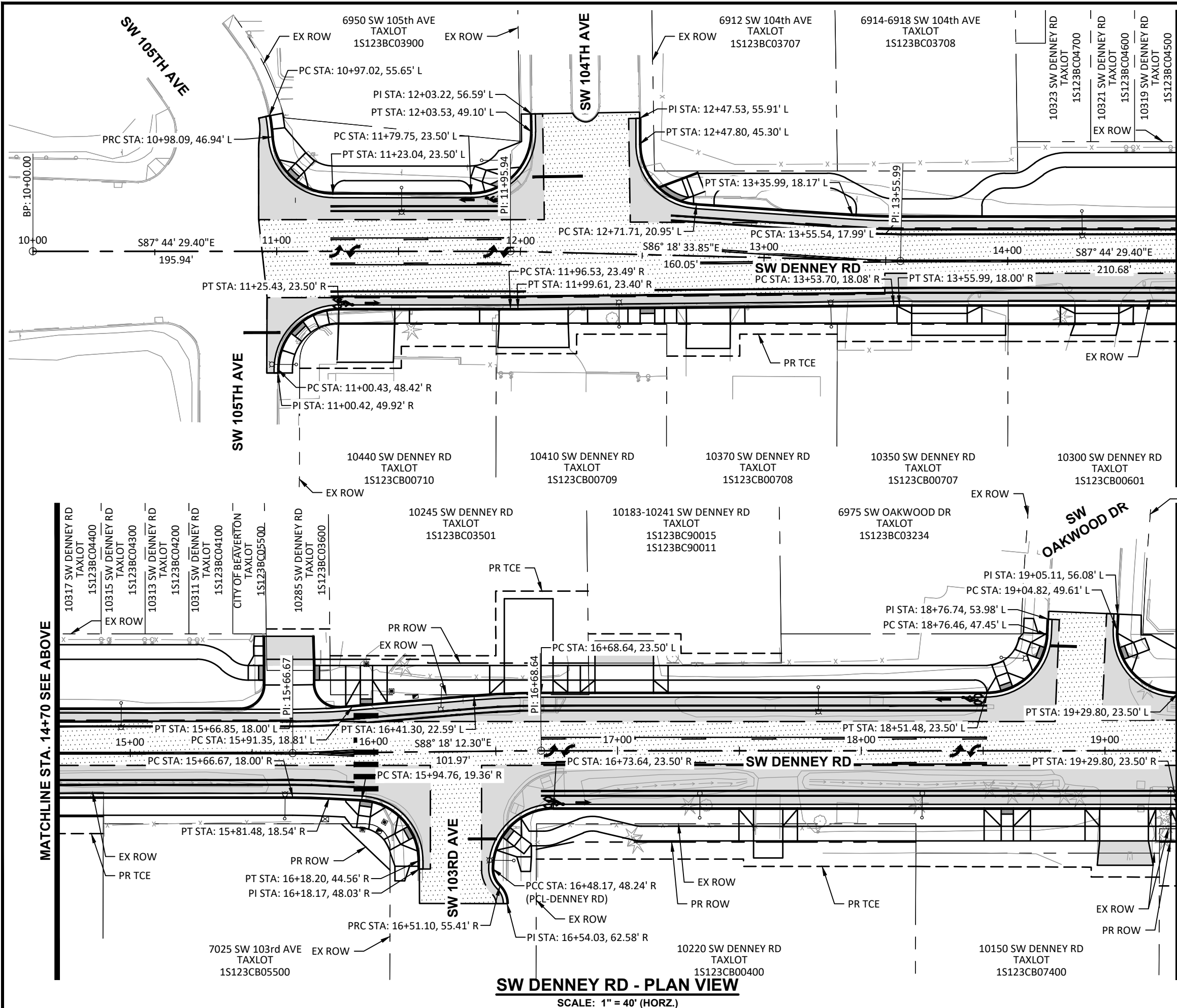
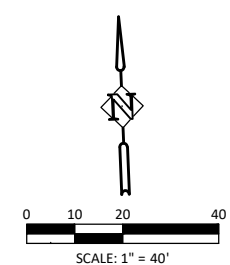
SW DENNEY RD STA POB TO 19+30

GEOMETRY AND PAVING PLANS

PROJECT NUMBER
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SHEET NO.
33 OF **84**

SHEET TITLE
2F



MATCHLINE STA. 14+70 SEE ABOVE

MATCHLINE STA. 19+30 SEE SHEET 2F-2

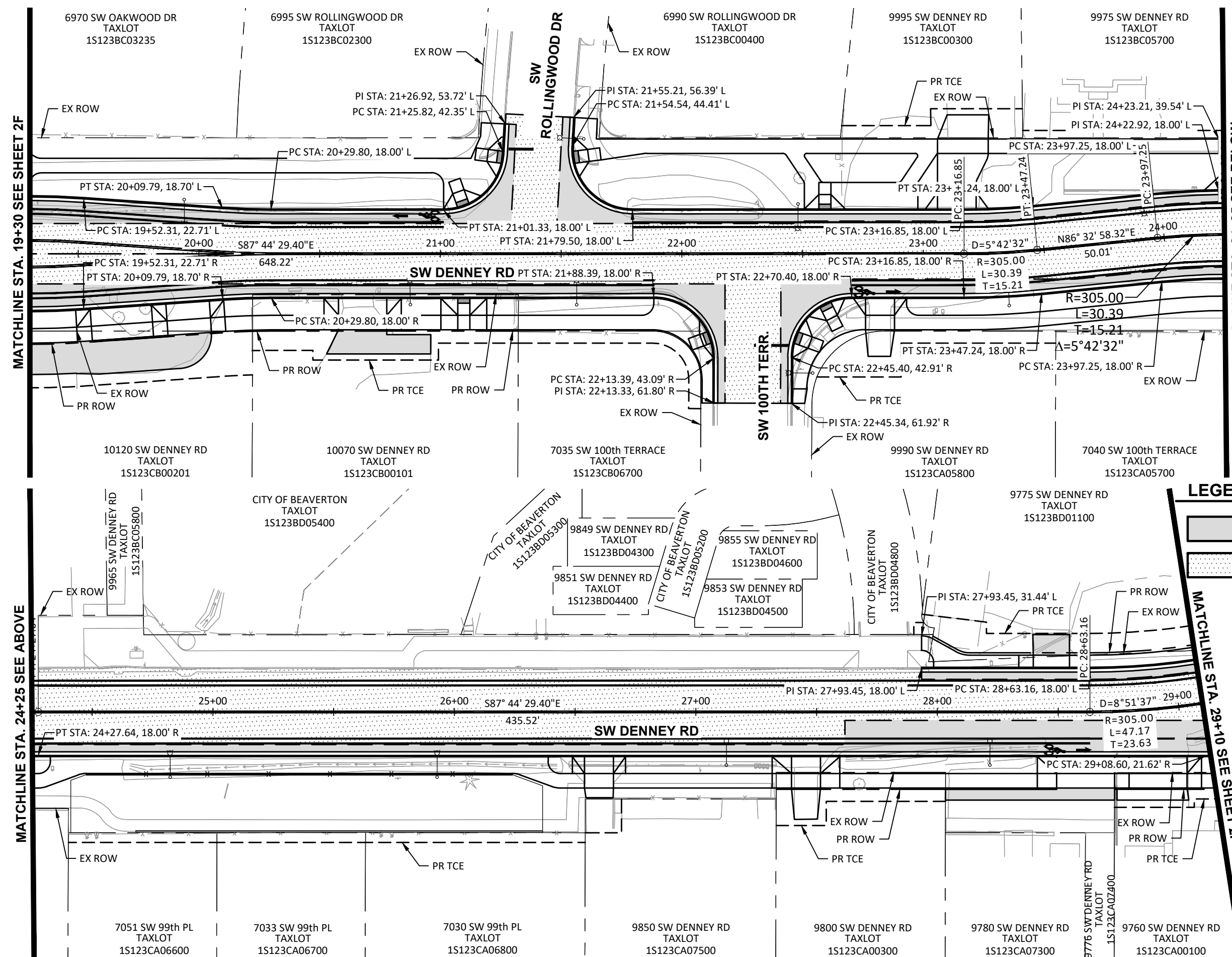
SW DENNEY RD - PLAN VIEW
 SCALE: 1" = 40' (HORZ.)



EXPIRES: 12/31/25

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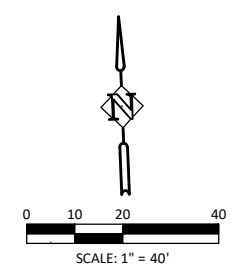


LEGEND

FULL DEPTH PAVEMENT SECTION
SEE TYPICAL SECTION FOR DETAILS

0 - 2" GRIND AND 2" INLAY
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NO.	REVISION
1	
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SW DENNEY RD - PLAN VIEW
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SW DENNEY ROAD
 SW 105TH AVE TO SW SCHOLLS FERRY ROAD
 WASHINGTON COUNTY

SW DENNEY RD STA 19+30 TO 29+10
GEOMETRY AND PAVING PLANS

PROJECT NUMBER
100666

SHEET NO.
34 OF 84

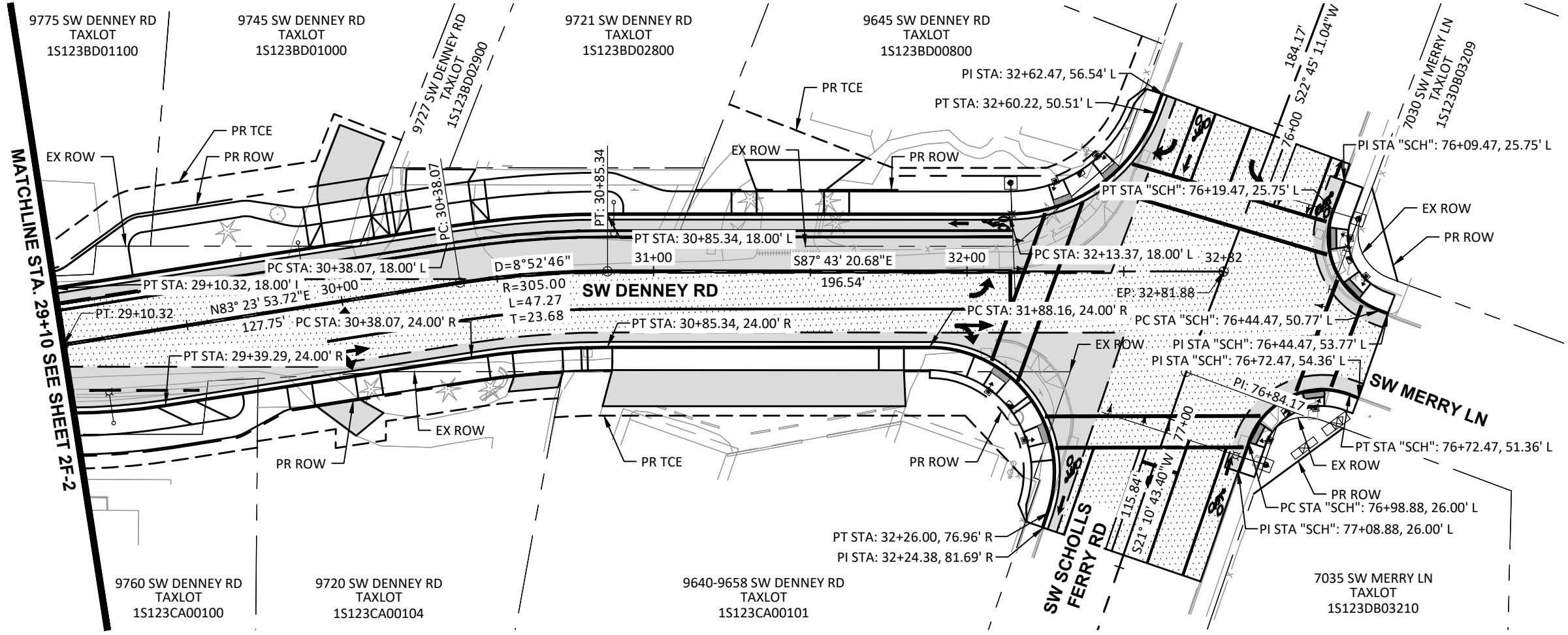
SHEET TITLE
2F-2



EXPIRES: 12/31/25

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Department of Land Use & Transportation Engineering

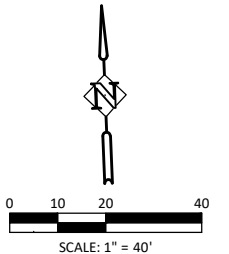


SW DENNEY RD - PLAN VIEW
 SCALE: 1" = 40' (HORZ.)

LEGEND

FULL DEPTH PAVEMENT SECTION
 SEE TYPICAL SECTION FOR DETAILS

0 - 2" GRIND AND 2" INLAY
 SEE TYPICAL SECTION FOR DETAILS



NO.	REVISION
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SW DENNEY ROAD
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SW DENNEY RD STA 29+10 TO EOP
GEOMETRY AND PAVING PLANS

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SHEET TITLE
2F-3

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Non NHS:	<input type="checkbox"/>		No	<input type="checkbox"/>

Design Exceptions for which version: 2007 <input type="checkbox"/> 2019 <input type="checkbox"/>		
<input type="checkbox"/> Design Speed	<input type="checkbox"/> Lane Width	<input type="checkbox"/> Bridge Rail
<input type="checkbox"/> Horizontal Alignment	<input type="checkbox"/> Parking Width	<input type="checkbox"/> Pavement Design Life
<input type="checkbox"/> Vertical Alignment	<input type="checkbox"/> Bike Lane/Multi-Use Path Width	<input type="checkbox"/> Vertical Clearance
<input type="checkbox"/> Intersection Sight Distance	<input type="checkbox"/> Median Width	<input type="checkbox"/> Super elevation
<input type="checkbox"/> Stopping Sight Distance	<input type="checkbox"/> Length of Cul-de-sac	<input type="checkbox"/> Super elevation Runoff
<input type="checkbox"/> Intersection / Driveway Spacing	<input type="checkbox"/> Shy Distance	<input type="checkbox"/> Clear Zone
<input type="checkbox"/> Pavement Cross Slope	<input type="checkbox"/> ADA Standards	<input type="checkbox"/> (Other)
<input type="checkbox"/> Grade	<input type="checkbox"/> Bridge Width	Sidewalk Width ¹

¹ Needs a Sidewalk Modification from Planning.

Description of Project:

Description of Exception:

Location of Design Feature:

Reasons For Not Attaining Standard: (Such As Cost/ Benefit, Crash History, Environmental, Etc.)

Effect on Other Standards:

Compatibility with Adjacent Sections and Context:

Mitigation for Exception Included In Design:

Supporting Documentation (Include the appropriate Plan Section, Cross Section, Alignments Sheets & Plan Details):

**CITY OF BEAVERTON
DESIGN EXCEPTION REQUEST**

Signatures

Prepared By: _____ **Date:** _____
(Engineer of Record)

Print Name:	_____	Phone:	_____
Company Name:	_____		
Company Address:	_____		
City:	_____	ST:	_____
Email Address:	_____		

Concurred By: _____ **Date:** _____
(Site Development Division Manager, Public Works Director,
OPS Program Manager, etc.)

(Print Name)

Concurred By: _____ **Date:** _____
(City Traffic Engineer)


(Print Name)

Approved By: _____ **Date:** _____
(City Engineer)

(Print Name)

PREPARED BY:

**ENGINEER OF RECORD
PROFESSIONAL
ENGINEER STAMP**



REGISTERED PROFESSIONAL
ENGINEER
70,863
Benjamin R. Austin
OREGON
JUL. 11, 2006
BENJAMIN R. AUSTIN

EXPIRES: 12/31/25

ADA Standards Design Exception Request – Additional Information

Description of Exception: The design team is requesting two design exceptions related to ADA standards, as outlined below:

1. Providing less than 6 ramps at a Tee Intersection rather than 6 ramps – as required by section 210.23.1-2f of the Engineering Design Manual
2. Providing 1 directional ramp serving crossings in two directions rather than two ramps – as required by section 210.23.1-2e of the Engineering Design Manual

Location of Design Feature: The design team is requesting design exceptions at the below locations:

1. Tee intersections with less than 6 ramps proposed:
 - a. SW Denney Rd and SW 104th Ave (3 ramps proposed)
 - b. SW Denney Rd and SW 103rd Ave (4 ramps proposed)
 - c. SW Denney Rd and SW Oakwood Dr (4 ramps proposed)
 - d. SW Denney Rd and SW Rollingwood Dr (4 ramps proposed)
 - e. SW Denney Rd and SW 100th Terr (4 ramps proposed)
2. Corners with 1 directional ramp serving two crossings:
 - a. The NE & SE corners of SW Denney Rd and SW 105th Ave
 - b. The NE corner of SW Denney Rd and SW 104th Ave

See the attached Roadway Plans for specific layouts.

Reason For Not Attaining Standard: The primary reason for not providing 6 ramps at all tee intersections is the impact that a full build would have on the adjacent private properties and existing, significant trees. At the intersections of SW Denney Rd and SW 104th Ave, SW 103rd Ave and SW Oakwood Dr, driveways would have to be closed or relocated to fit the additional ramps to meet the design standard of 6 ramps. The impact this would have on the private properties would be significant. Adding additional ramps at the intersections of SW Denney Rd and SW Rollingwood Dr, and SW 100th Terrace would require the removal of three significant grove trees, that were identified as a priority for this project to preserve. All tee intersections listed are in close proximity to other intersections, between 90' and 250' away, lessening the impact of fewer crossings than required.

Similarly to the design exception related to tee intersections, construction of dual ramps at the locations listed above would impact adjacent driveways and would push the crossings into a skewed position. The singular ramps proposed at SW 105th Ave match the recently constructed ODOT owned ramps on the opposite side of the intersection.

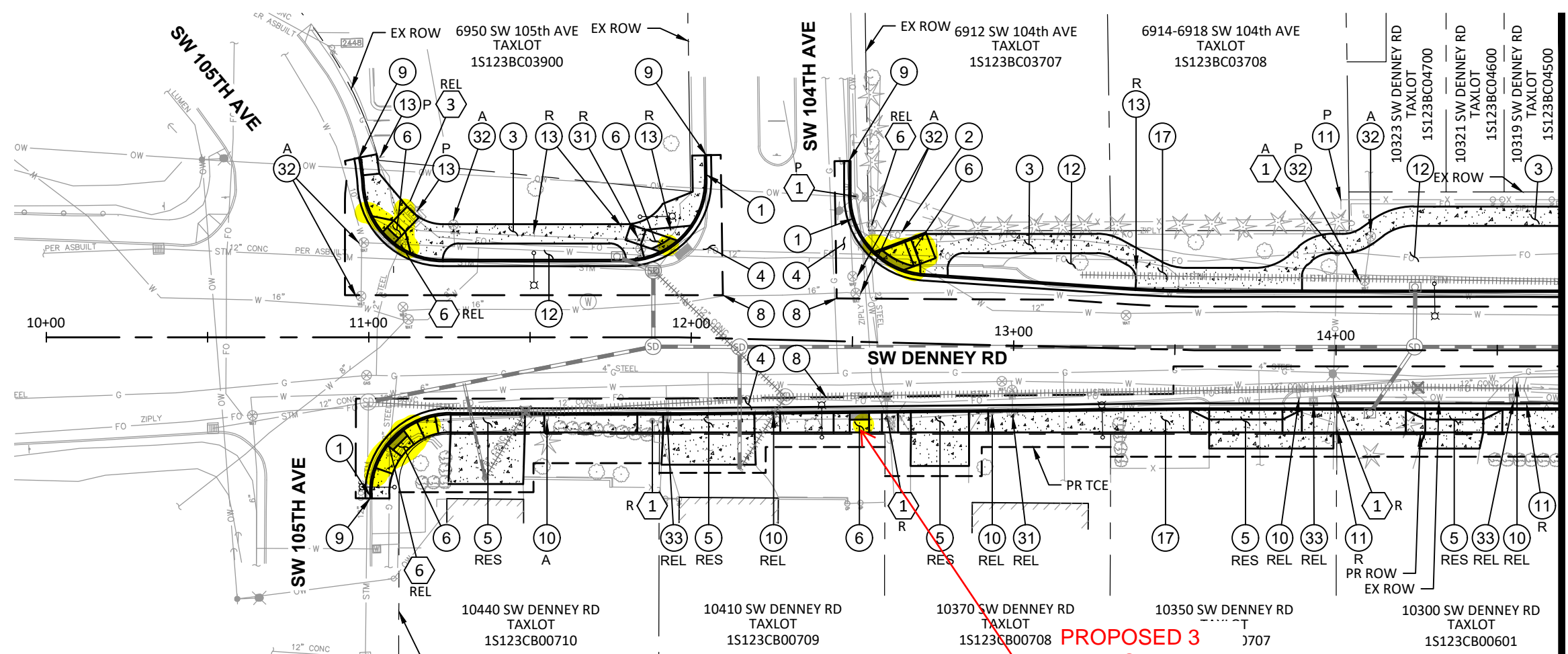
Compatibility with Adjacent Sections and Context: Single ramps serving multiple crossings as well as tee intersections with less than 6 ramps is consistent with the existing conditions in the ODOT right of way at the HWY 217 overpass, as well as further west along SW Denney Rd.



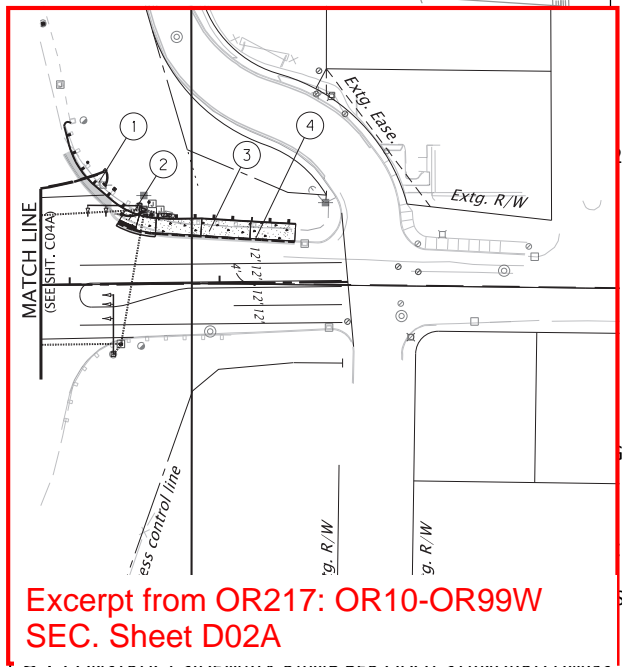
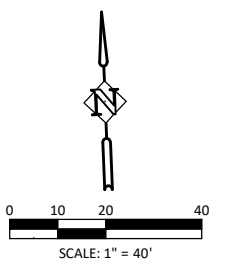
EXPIRES: 12/31/25

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Department of Land Use & Transportation Engineering



SW DENNEY RD - PLAN VIEW
 SCALE: 1" = 40' (HORZ.)



- 8 SAWCUT AND REMOVE EXISTING SURFACE, AS REQUIRED AND DIRECTED.
- 9 MATCH NEW CURB TO EXISTING CURB AND/OR MATCH NEW SIDEWALK TO EXISTING SIDEWALK, AS REQUIRED AND DIRECTED. PROTECT EXISTING CURB AND/OR CONCRETE SIDEWALK.

- 10 RELOCATE (REL), REMOVE (R), ADJUST (A) OR PROTECT (P) EXISTING MAILBOX(ES). FOR ANY RELOCATION OR ADJUSTMENT, SEE ODOT STANDARD DRAWING RD100 AND RD101 ON SHEETS 2B-12 AND 2B-13. MAILBOX LOCATIONS PER CITY STANDARD DWG 140 ON SHEET 2B-5. COORDINATE LOCATION WITH ENGINEER.
- 11 REMOVE (R), ADJUST (A), OR PROTECT (P) EXISTING FENCE & GATES. FOR REMOVALS, REMOVE FENCE WITHIN RIGHT-OF-WAY LIMITS.
- 12 CONSTRUCT LANDSCAPE AREA. SEE LANDSCAPING PLANS SHEET SERIES "LA" FOR LIMITS AND DETAILS.
- 13 REMOVE (R), ADJUST (A), OR PROTECT (P) EXISTING SIGN.
- 17 CONSTRUCT CONCRETE CURB TIGHT SIDEWALK PER CITY STANDARD DWG 216 ON SHEET 2B-8.
- 31 RELOCATE (REL), ADJUST (A), OR PROTECT (P) EXISTING FIRE HYDRANT. RELOCATIONS AND ADJUSTMENTS BY OTHERS. CONTRACTOR TO COORDINATE.
- 32 RELOCATE (REL), ADJUST (A), OR PROTECT (P) EXISTING WATER VALVE. RELOCATIONS AND ADJUSTMENTS BY OTHERS. CONTRACTOR TO COORDINATE.
- 33 RELOCATE (REL), ADJUST (A), OR PROTECT (P) EXISTING WATER METER, BOX, AND SERVICE. RELOCATIONS AND ADJUSTMENTS BY OTHERS. CONTRACTOR TO COORDINATE.

GENERAL INFORMATION NOTES:

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- 6 RELOCATE (REL), ADJUST (A), OR PROTECT (P) EXISTING GAS VALVE. RELOCATION OR ADJUSTMENT BY UTILITY. CONTRACTOR TO COORDINATE.

PROPOSED DESIGN MODIFICATIONS:

- 6' CURB TIGHT SIDEWALK - 11+25.43 RT. TO 13+55.99 RT. TO AVOID IMPACTS TO ADJACENT PRIVATE IMPROVEMENTS.
- 6' CURB TIGHT SIDEWALK - 13+55.99 LT. TO 13+89.46 LT. TO AVOID TREE IMPACTS.
- 11' TRAVEL LANES - 11+25.43 TO 32+13.86 TO ACCOMMODATE THE NEEDED MULTI-MODAL IMPROVEMENTS WHILE MINIMIZING IMPACT TO TREES AND ADJACENT PROPERTIES.
- NE CORNER OF SE DENNEY RD AND SE 105TH AVE - SINGLE ADA RAMP TO ALIGN WITH ADJACENT CORNERS.
- SE CORNER OF SE DENNEY RD AND SE 105TH AVE - SINGLE ADA RAMP TO ALIGN WITH RECEIVING RAMP AND AVOID PRIVATE PROPERTY IMPACTS.
- NW CORNER OF SE DENNEY RD AND SE 104TH AVE - SINGLE DIRECTIONAL ADA RAMP TO ALIGN WITH RECEIVING RAMP ON EAST SIDE OF INTERSECTION. DRIVEWAYS ON THE SOUTH SIDE PRECLUDE A RAMP AT THIS INTERSECTION.
- NE CORNER OF SE DENNEY RD AND SE 104TH AVE - SINGLE ADA RAMP TO SERVE BOTH CROSSINGS DUE TO LIMITED RIGHT OF WAY AND PLACEMENT OF RAMP ON THE SOUTH SIDE DUE TO DRIVEWAY LOCATIONS.

NO.	REVISION
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SW DENNEY ROAD
 SW 105TH AVE TO SW SCHOLLS FERRY ROAD
 WASHINGTON COUNTY

SW DENNEY RD STA POB TO 14+70
ROADWAY PLAN AND NOTES

PROJECT NUMBER
100666

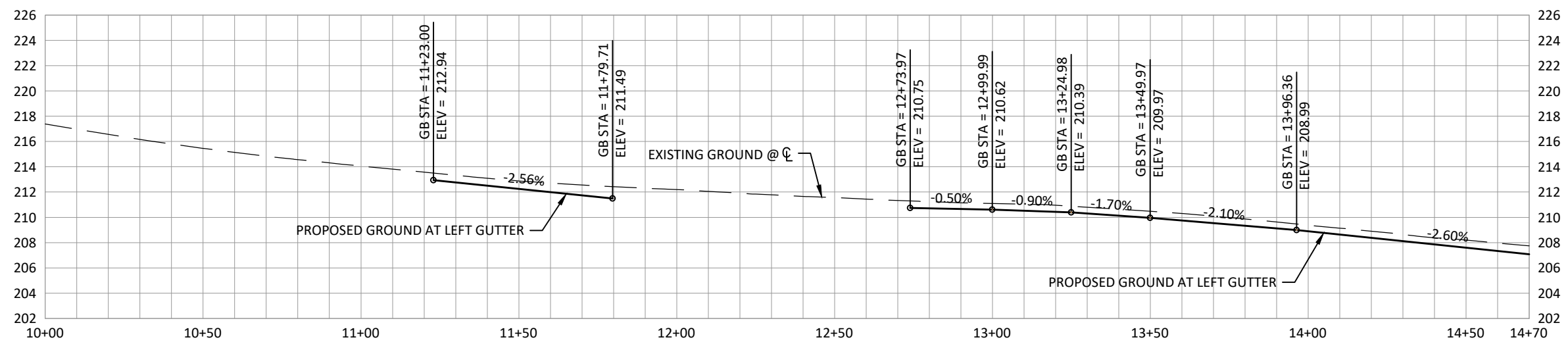
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 39 OF 84
 SHEET TITLE
3



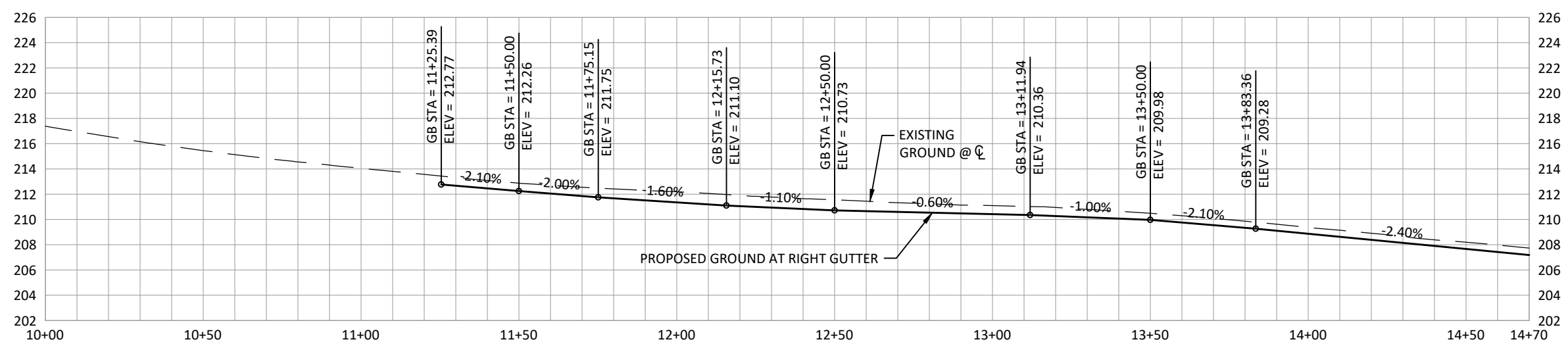
EXPIRES: 12/31/25

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 Phone: 503.221.1131 www.hhpri.com Fax: 503.221.1171

Department of Land Use & Transportation Engineering



SW DENNEY RD - LEFT PROFILE
 SCALE: 1" = 40' (HORZ.)
 1" = 10' (VERT.)



SW DENNEY RD - RIGHT PROFILE
 SCALE: 1" = 40' (HORZ.)
 1" = 10' (VERT.)

NO.	REVISION
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SW DENNEY ROAD
 SW 105TH AVE TO SW SCHOLLS FERRY ROAD
 WASHINGTON COUNTY

ROADWAY PROFILES

PROJECT NUMBER
100666

SHEET NO.
 40 OF 84

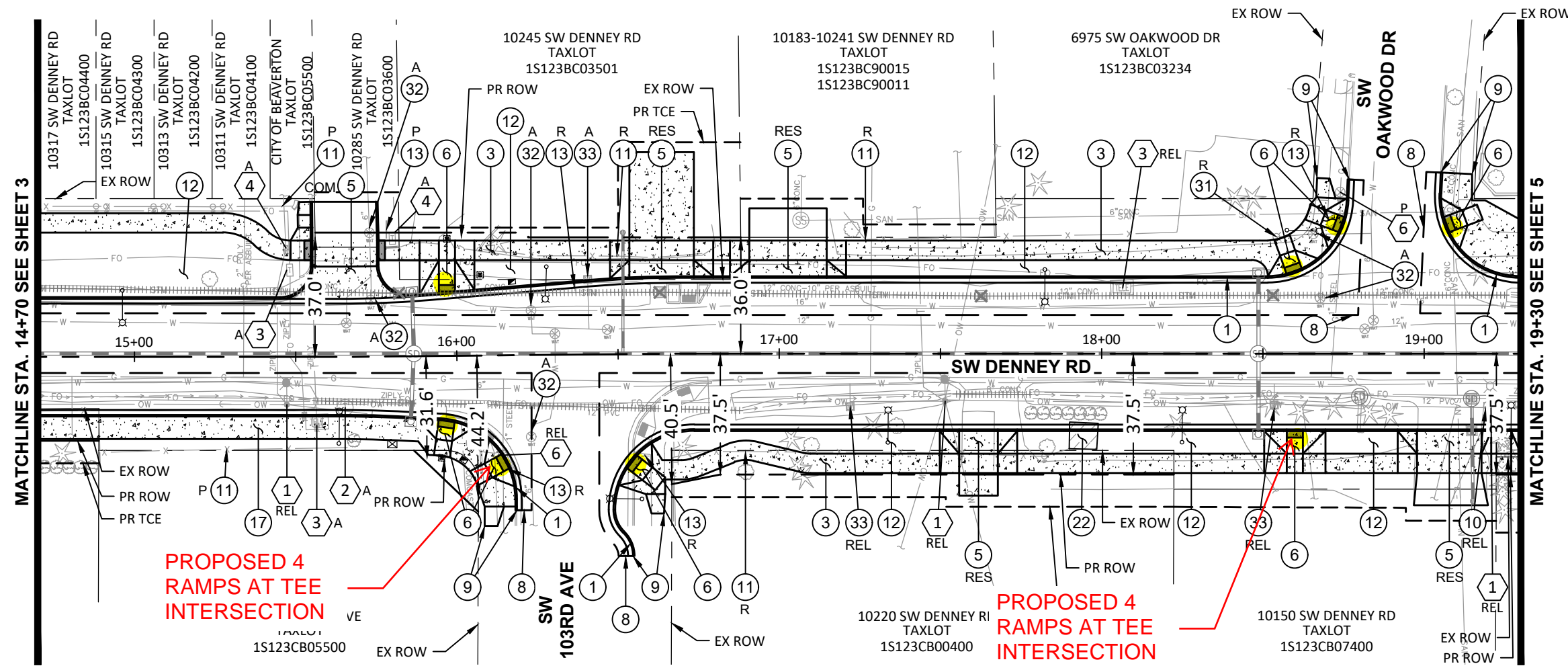
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EXPIRES: 12/31/25

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Department of Land Use & Transportation Engineering



SW DENNEY RD - PLAN VIEW
 SCALE: 1" = 40' (HORZ.)

CONSTRUCTION NOTES:

- 1 CONSTRUCT CURB AND GUTTER PER CITY STANDARD DWG 205 ON SHEET 2B-6.
- 3 CONSTRUCT CONCRETE STANDARD SIDEWALK PER CITY STANDARD DWG 215 ON SHEET 2B-8.
- 5 CONSTRUCT RESIDENTIAL (RES) OR COMMERCIAL (COM) CONCRETE DRIVEWAY APPROACH PER CITY STANDARD DWGS 210 AND 211 ON SHEET 2B-7. MATCH EXISTING WIDTH AND MATERIAL BEHIND APPROACH UNLESS NOTED OTHERWISE. SAWCUT AND REMOVE EXISTING SURFACE AS REQUIRED. MINIMUM STRUCTURAL SECTION BEHIND APPROACH: 4" AC PAVEMENT OVER 8" AGGREGATE BASE OR 6" P.C.C. OVER 6" AGGREGATE BASE. SEE DRIVEWAY GRADE ELEVATIONS ON SHEET SERIES "XX".
- 6 CONSTRUCT SIDEWALK RAMP PER ODOT STANDARD DWGS RD902, RD904, RD905, RD910, RD912, RD913, RD920 AND RD922 ON SHEETS 2B-15 THRU 2B-22. SEE CURB RETURN GRADING ELEVATIONS ON SHEET SERIES "XX".
- 8 SAWCUT AND REMOVE EXISTING SURFACE, AS REQUIRED AND DIRECTED.
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- 17 CONSTRUCT CONCRETE CURB TIGHT SIDEWALK PER CITY STANDARD DWG 216 ON SHEET 2B-8.
- 22 REMOVE PRIVATE IMPROVEMENTS.
- 31 RELOCATE (REL), ADJUST (A), OR PROTECT (P) EXISTING FIRE HYDRANT. RELOCATIONS AND ADJUSTMENTS BY OTHERS. CONTRACTOR TO COORDINATE.
- 32 RELOCATE (REL), ADJUST (A). OR PROTECT (P) EXISTING WATER VALVE. RELOCATIONS AND ADJUSTMENTS BY OTHERS. CONTRACTOR TO COORDINATE.
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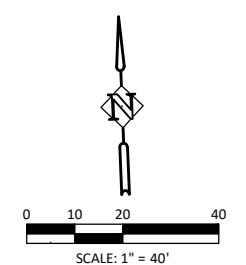
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- 4 RELOCATE (REL), ADJUST (A), OR PROTECT (P) EXISTING UTILITY PEDESTAL. RELOCATION OR ADJUSTMENT BY UTILITY. CONTRACTOR TO COORDINATE.
- 6 RELOCATE (REL), ADJUST (A), OR PROTECT (P) EXISTING GAS VALVE. RELOCATION OR ADJUSTMENT BY UTILITY. CONTRACTOR TO COORDINATE.

PROPOSED DESIGN MODIFICATIONS:

WEST SIDE OF SW DENNEY RD AND SW 103RD AVE - NO PEDESTRIAN RAMP ON NORTH SIDE DUE TO CONFLICT WITH DRIVEWAY AND DESIRE TO ENCOURAGE PEDESTRIANS TO USE THE RRFB PEDESTRIAN CROSSING ON THE WEST LEG.

EAST SIDE OF SW DENNEY RD AND SW OAKWOOD DR - NO PEDESTRIAN RAMPS ON EAST SIDE OF INTERSECTION DUE TO CONFLICT WITH DRIVEWAY.



NO.	REVISION
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SW DENNEY ROAD
 SW 105TH AVE TO SW SCHOLLS FERRY ROAD
 WASHINGTON COUNTY

SW DENNEY RD STA 14+70 TO 19+30
ROADWAY PLAN AND NOTES

PROJECT NUMBER
100666

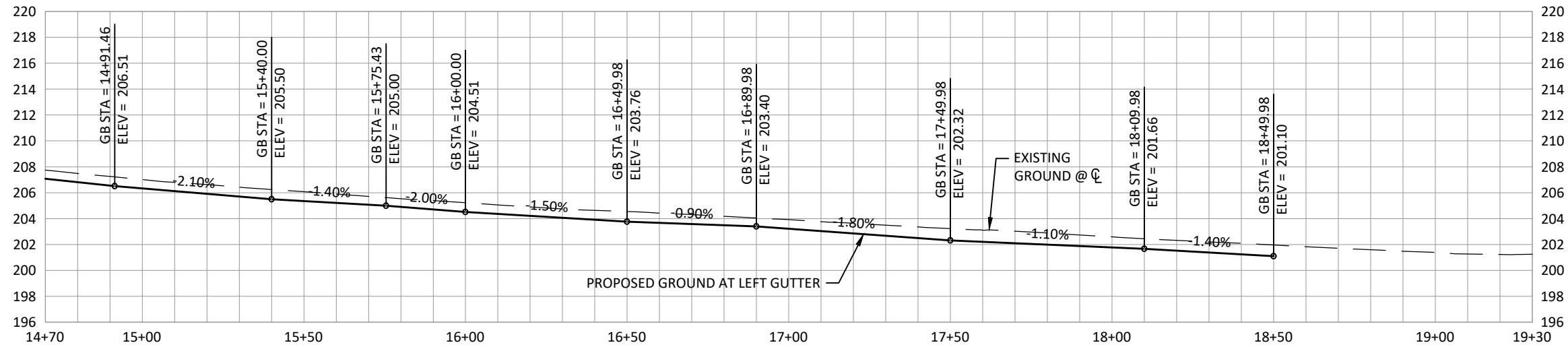
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 41 OF 84
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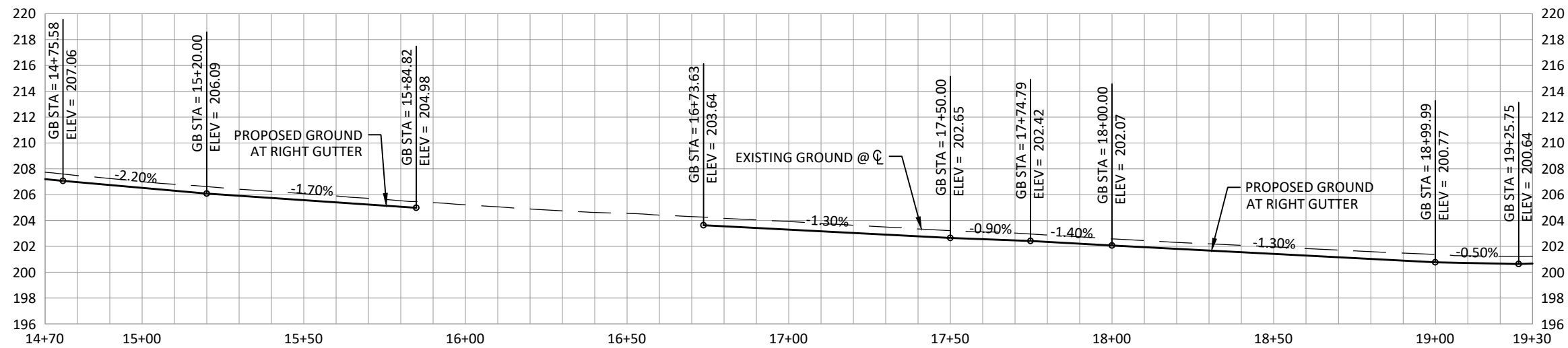
EXPIRES: 12/31/25

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Department of Land Use & Transportation Engineering



SW DENNEY RD - LEFT PROFILE
 SCALE: 1" = 40' (HORZ.)
 1" = 10' (VERT.)



SW DENNEY RD - RIGHT PROFILE
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 1" = 10' (VERT.)

NO.	REVISION
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SW DENNEY ROAD
 SW 105TH AVE TO SW SCHOLLS FERRY ROAD
 WASHINGTON COUNTY

ROADWAY PROFILES

PROJECT NUMBER
100666

SHEET NO.
 42 OF 84

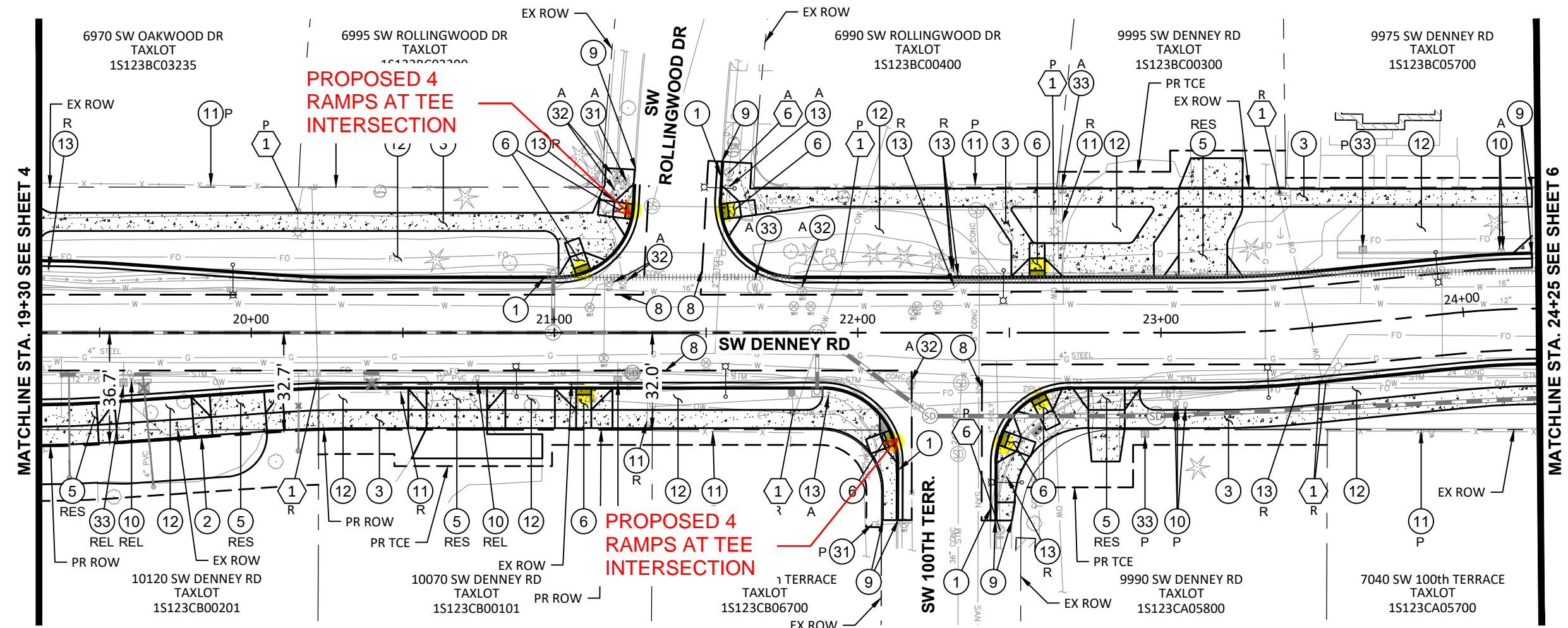
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EXPIRES: 12/31/25

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SW DENNEY RD - PLAN VIEW
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CONSTRUCTION NOTES:

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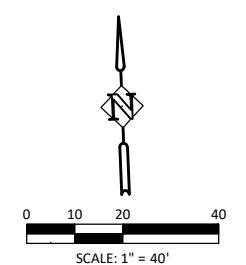
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PROPOSED DESIGN MODIFICATIONS:

SW ROLLINGWOOD DR AND SW 100TH TERR AT SW DENNEY RD ACT AS AN OFFSET INTERSECTION. NO ADA RAMPS PROVIDED ON INTERIOR CROSSING DUE TO CONFLICT WITH SIGNIFICANT TREES ON THE NORTH SIDE.



NO.	REVISION
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SW DENNEY ROAD
 SW 105TH AVE TO SW SCHOLLS FERRY ROAD
 WASHINGTON COUNTY

SW DENNEY RD STA 19+30 TO 24+25
ROADWAY PLAN AND NOTES

PROJECT NUMBER
100666

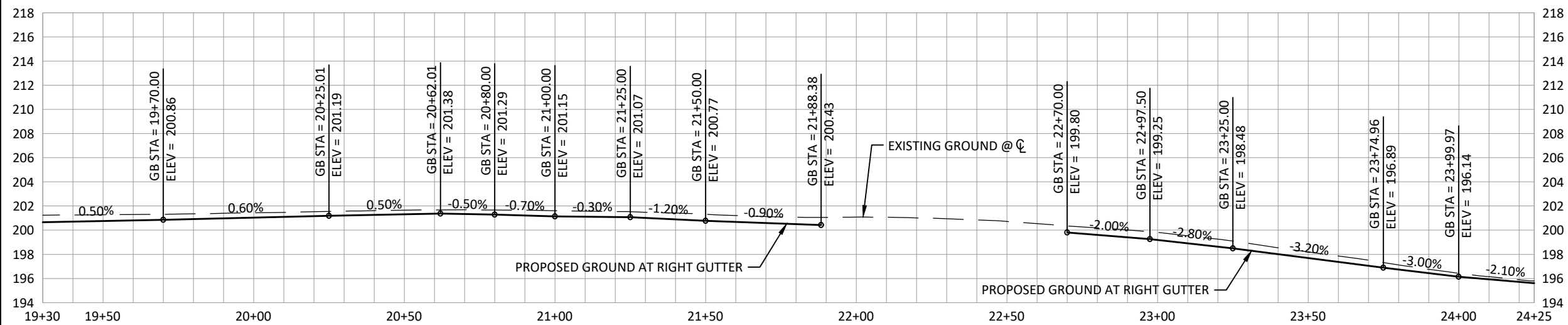
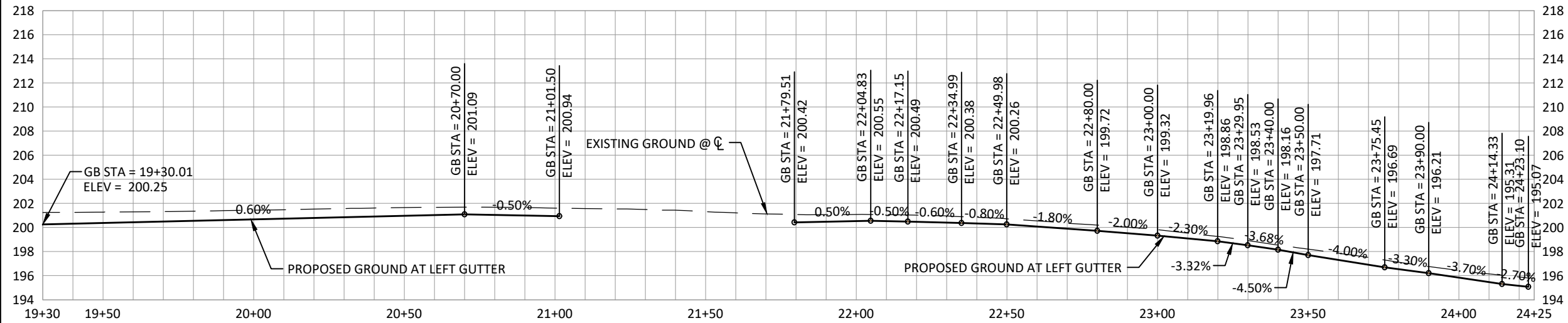
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43 OF **84**
 SHEET TITLE
5



EXPIRES: 12/31/25

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Department of Land Use & Transportation Engineering



NO.	REVISION
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SW DENNEY ROAD
 SW 105TH AVE TO SW SCHOLLS FERRY ROAD
 WASHINGTON COUNTY

ROADWAY PROFILES

PROJECT NUMBER
100666

SHEET NO.
 44 OF 84

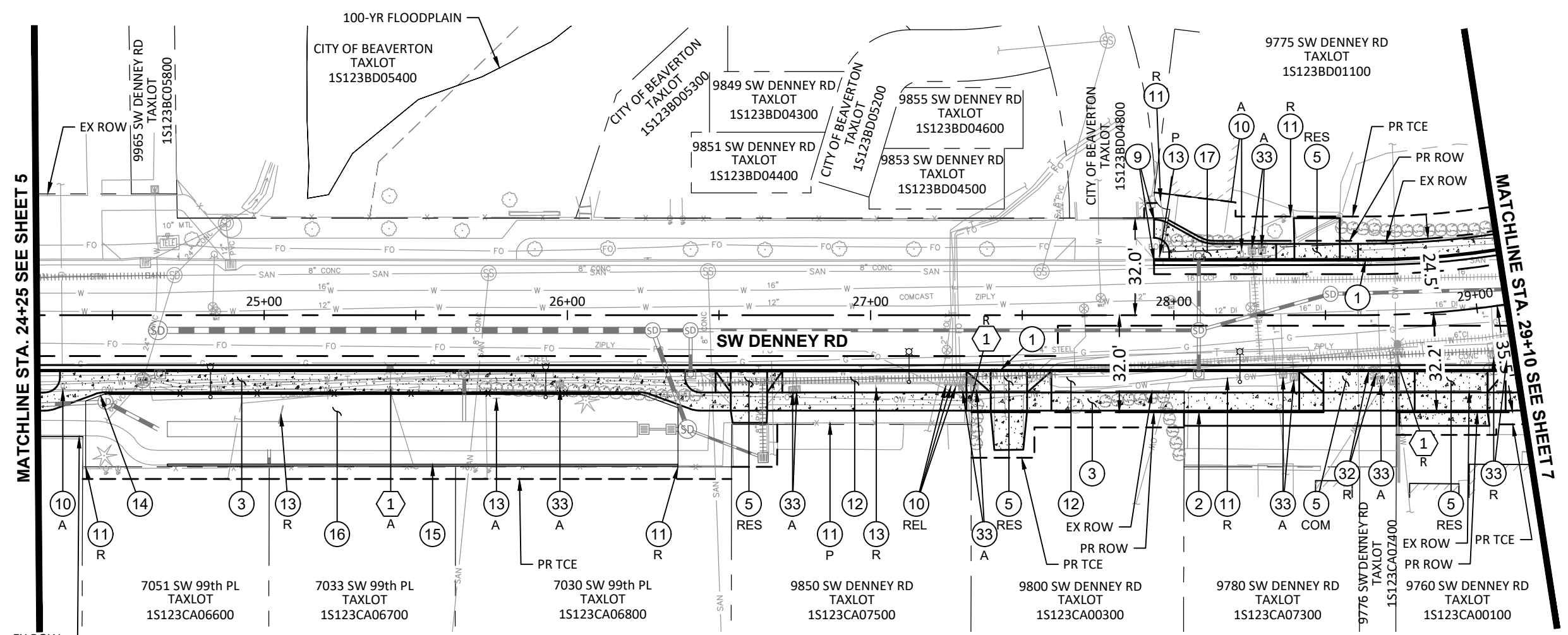
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EXPIRES: 12/31/25

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SW DENNEY RD - PLAN VIEW
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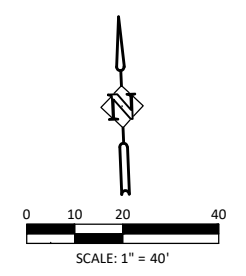
- 12 CONSTRUCT LANDSCAPE AREA. SEE LANDSCAPING PLANS SHEET SERIES "LA" FOR LIMITS AND DETAILS.
- 13 REMOVE (R), ADJUST (A). OR PROTECT (P) EXISTING SIGN.
- 14 INSTALL CL-6R CHAIN LINK FENCE WITH BLACK VINYL COATING PER ODOT STANDARD DWGS RD815 ON SHEET 2B-14.
- 15 CONSTRUCT WALL. SEE SHEET XX FOR WALL PLANS AND SHEET XX FOR DETAILS.
- 16 CONSTRUCT STORMWATER FACILITY PER DETAILS ON SHEET 4A-2.
- 17 CONSTRUCT CONCRETE CURB TIGHT SIDEWALK PER CITY STANDARD DWG 216 ON SHEET 2B-8.
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- 33 RELOCATE (REL), ADJUST (A). OR PROTECT (P) EXISTING WATER METER, BOX, AND SERVICE. RELOCATIONS AND ADJUSTMENTS BY OTHERS. CONTRACTOR TO COORDINATE.

GENERAL INFORMATION NOTES:

- 1 REMOVE OR RELOCATE (R), OR PROTECT (P) EXISTING UTILITY POLE. REMOVAL OR RELOCATION BY UTILITY. CONTRACTOR TO COORDINATE POLE AND WIRE RELOCATIONS WITH APPROPRIATE UTILITIES.

PROPOSED DESIGN MODIFICATIONS:

- 6' CURB TIGHT SIDEWALK - 24+32.64 RT. TO 26+38.85 RT. TO PROVIDE SPACE FOR AN EXTENDED DRY POND STORMWATER FACILITY.
- 6' CURB TIGHT SIDEWALK - 28+63.16 RT. TO 29+35.32 RT. TO AVOID IMPACTS TO TREES.



NO.	REVISION
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SW DENNEY ROAD
SW 105TH AVE TO SW SCHOLLS FERRY ROAD
WASHINGTON COUNTY

SW DENNEY RD STA. 24+25 TO 29+10
ROADWAY PLAN AND NOTES

PROJECT NUMBER
100666

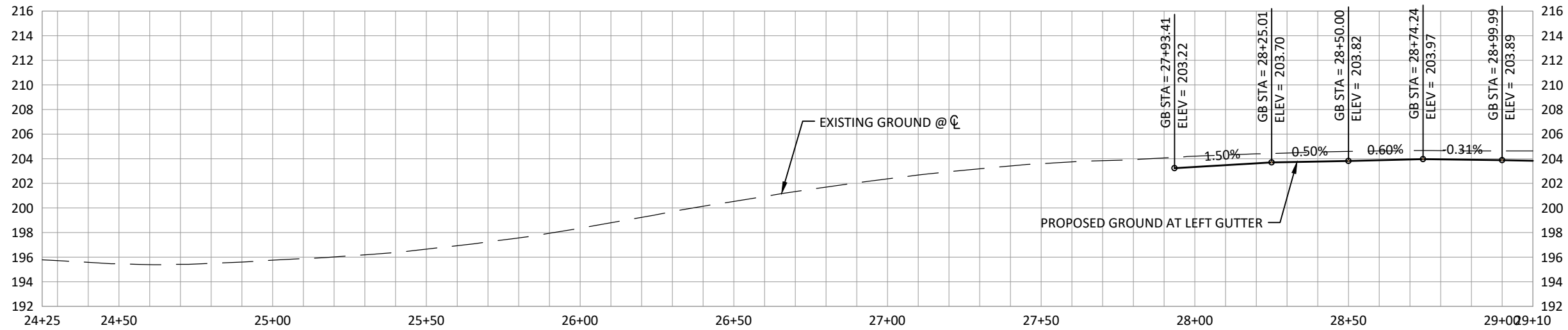
SHEET NO.
45 OF 84
SHEET TITLE
6



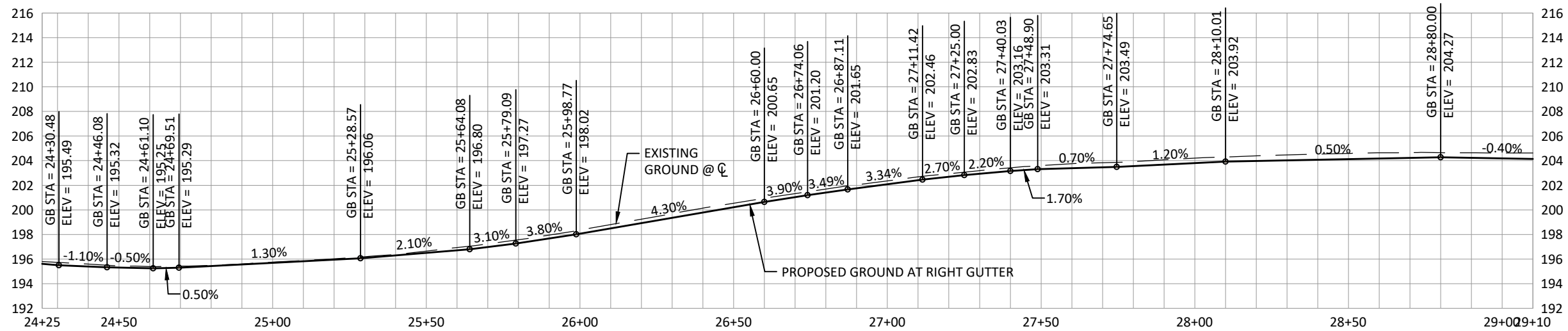
EXPIRES: 12/31/25

Harper Houf Peterson Righellis Inc.
 ENGINEERS * PLANNERS
 LANDSCAPE ARCHITECTS * SURVEYORS
 205 SE Spokane Street, Suite 300, Portland, OR 97202
 Phone: 503.221.1131 www.hhpri.com Fax: 503.221.1171

Department of Land Use & Transportation Engineering
 WASHINGTON COUNTY OREGON



SW DENNEY RD - LEFT PROFILE
 SCALE: 1" = 40' (HORZ.)
 1" = 10' (VERT.)



SW DENNEY RD - RIGHT PROFILE
 SCALE: 1" = 40' (HORZ.)
 1" = 10' (VERT.)

NO.	REVISION
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SW DENNEY ROAD
 SW 105TH AVE TO SW SCHOLLS FERRY ROAD
 WASHINGTON COUNTY

ROADWAY PROFILES

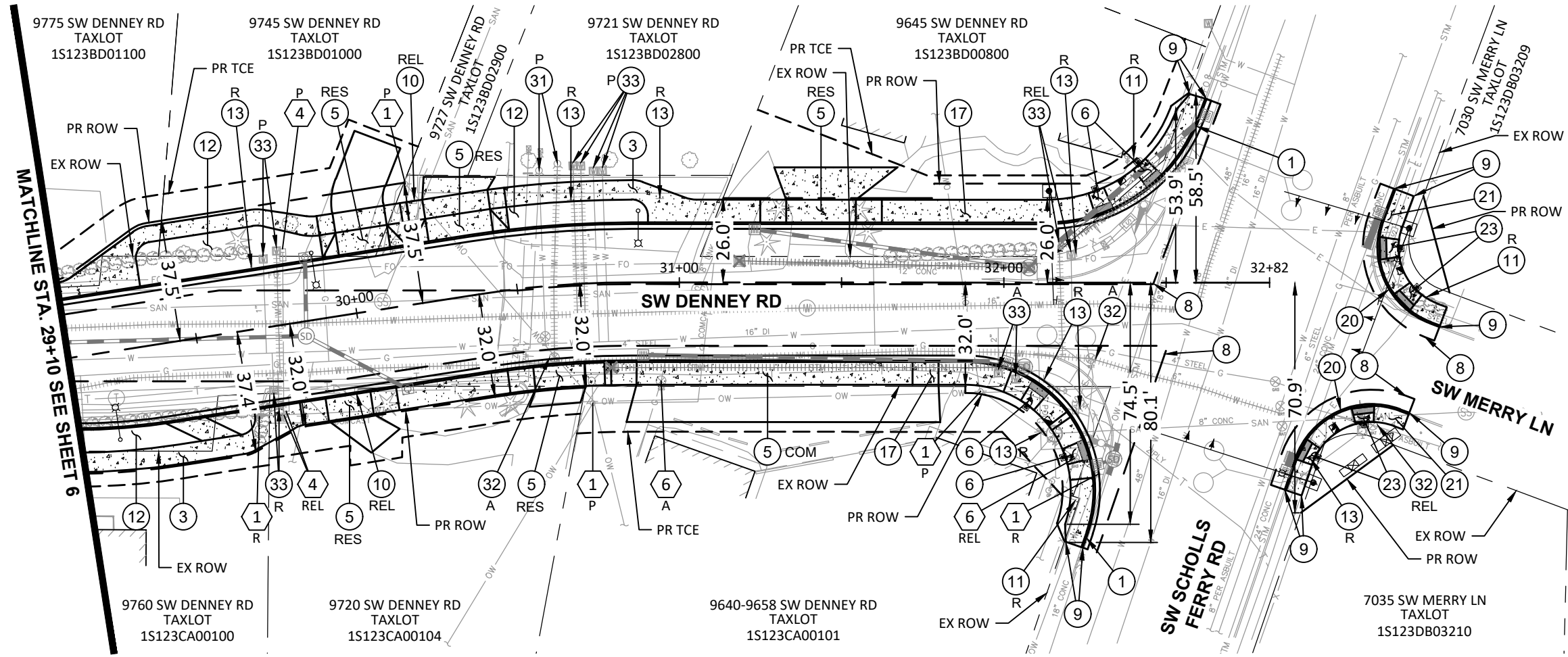
PROJECT NUMBER
100666
 SHEET NO.
 46 OF 84
 SHEET TITLE
6A



EXPIRES: 12/31/25

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SW DENNEY RD - PLAN VIEW
SCALE: 1" = 40' (HORZ.)

CONSTRUCTION NOTES:

- 1 CONSTRUCT CURB AND GUTTER PER CITY STANDARD DWG 205 ON SHEET 2B-6.
- 3 CONSTRUCT CONCRETE STANDARD SIDEWALK PER CITY STANDARD DWG 215 ON SHEET 2B-8.
- 5 CONSTRUCT RESIDENTIAL (RES) OR COMMERCIAL (COM) CONCRETE DRIVEWAY APPROACH PER CITY STANDARD DWGS 210 AND 211 ON SHEET 2B-7. MATCH EXISTING WIDTH AND MATERIAL BEHIND APPROACH UNLESS NOTED OTHERWISE. SAWCUT AND REMOVE EXISTING SURFACE AS REQUIRED. MINIMUM STRUCTURAL SECTION BEHIND APPROACH: 4" AC PAVEMENT OVER 8" AGGREGATE BASE OR 6" P.C.C. OVER 6" AGGREGATE BASE. SEE DRIVEWAY GRADE ELEVATIONS ON SHEET SERIES "XX".
- 6 CONSTRUCT SIDEWALK RAMP PER ODOT STANDARD DWGS RD902, RD904, RD905, RD910, RD912, RD913, RD920 AND RD922 ON SHEETS 2B-15 THRU 2B-22. SEE CURB RETURN GRADING ELEVATIONS ON SHEET SERIES "XX".
- 8 SAWCUT AND REMOVE EXISTING SURFACE, AS REQUIRED AND DIRECTED.
- 9 MATCH NEW CURB TO EXISTING CURB AND/OR MATCH NEW SIDEWALK TO EXISTING SIDEWALK, AS REQUIRED AND DIRECTED. PROTECT EXISTING CURB AND/OR CONCRETE SIDEWALK.
- 10 RELOCATE (REL), REMOVE (R), ADJUST (A) OR PROTECT (P) EXISTING MAILBOX(ES). FOR ANY RELOCATION OR ADJUSTMENT, SEE ODOT STANDARD DRAWING RD100 AND RD101 ON SHEETS 2B-12 AND 2B-13. MAILBOX LOCATIONS PER CITY STANDARD DWG 140 ON SHEET 2B-5. COORDINATE LOCATION WITH ENGINEER.

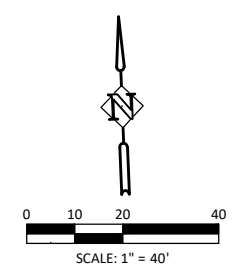
- 11 REMOVE (R), ADJUST (A). OR PROTECT (P) EXISTING FENCE & GATES. FOR REMOVALS, REMOVE FENCE WITHIN RIGHT-OF-WAY LIMITS.
- 12 CONSTRUCT LANDSCAPE AREA. SEE LANDSCAPING PLANS SHEET SERIES "LA" FOR LIMITS AND DETAILS.
- 13 REMOVE (R), ADJUST (A). OR PROTECT (P) EXISTING SIGN.
- 17 CONSTRUCT CONCRETE CURB TIGHT SIDEWALK PER CITY STANDARD DWG 216 ON SHEET 2B-8.
- 20 CONSTRUCT STANDARD CURB PER WASHINGTON COUNTY STANDARD DWG 2010 ON SHEET 2B-23.
- 21 CONSTRUCT CONCRETE STANDARD SIDEWALK PER WASHINGTON COUNTY STANDARD DWG 2110 ON SHEET 2B-23.
- 23 CONSTRUCT SIDEWALK RAMP PER ODOT STANDARD DWGS RD902, RD904, RD905, RD910, RD912, RD913, RD920 AND RD922 SHEETS 2B-15 THRU 2B-22 AND WASHINGTON COUNTY STANDARD DWG 2130 ON SHEET 2B-24. SEE CURB RETURN GRADING ELEVATIONS ON SHEET SERIES "XX".
- 31 RELOCATE (REL), ADJUST (A), OR PROTECT (P) EXISTING FIRE HYDRANT. RELOCATIONS AND ADJUSTMENTS BY OTHERS. CONTRACTOR TO COORDINATE.
- 32 RELOCATE (REL), ADJUST (A). OR PROTECT (P) EXISTING WATER VALVE. RELOCATIONS AND ADJUSTMENTS BY OTHERS. CONTRACTOR TO COORDINATE.
- 33 RELOCATE (REL), ADJUST (A). OR PROTECT (P) EXISTING WATER METER, BOX, AND SERVICE. RELOCATIONS AND ADJUSTMENTS BY OTHERS. CONTRACTOR TO COORDINATE.

GENERAL INFORMATION NOTES:

- 1 REMOVE OR RELOCATE (R), OR PROTECT (P) EXISTING UTILITY POLE. REMOVAL OR RELOCATION BY UTILITY. CONTRACTOR TO COORDINATE POLE AND WIRE RELOCATIONS WITH APPROPRIATE UTILITIES.
- 4 RELOCATE (REL), ADJUST (A), OR PROTECT (P) EXISTING UTILITY PEDESTAL. RELOCATION OR ADJUSTMENT BY UTILITY. CONTRACTOR TO COORDINATE.
- 6 RELOCATE (REL), ADJUST (A), OR PROTECT (P) EXISTING GAS VALVE. RELOCATION OR ADJUSTMENT BY UTILITY. CONTRACTOR TO COORDINATE.

PROPOSED DESIGN MODIFICATIONS:

- 6' CURB TIGHT SIDEWALK - 29+64.11 RT. TO 32+13.86 RT. TO AVOID IMPACTS TO TREES AND PRIVATE IMPROVEMENTS.
- 6' CURB TIGHT SIDEWALK - 30+90.34 LT. TO 32+13.86 LT. TO AVOID IMPACTS TO TREES AND PRIVATE IMPROVEMENTS.



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SW DENNEY ROAD
SW 105TH AVE TO SW SCHOLLS FERRY ROAD
WASHINGTON COUNTY

SW DENNEY RD STA 29+10 TO EOP
ROADWAY PLAN AND NOTES

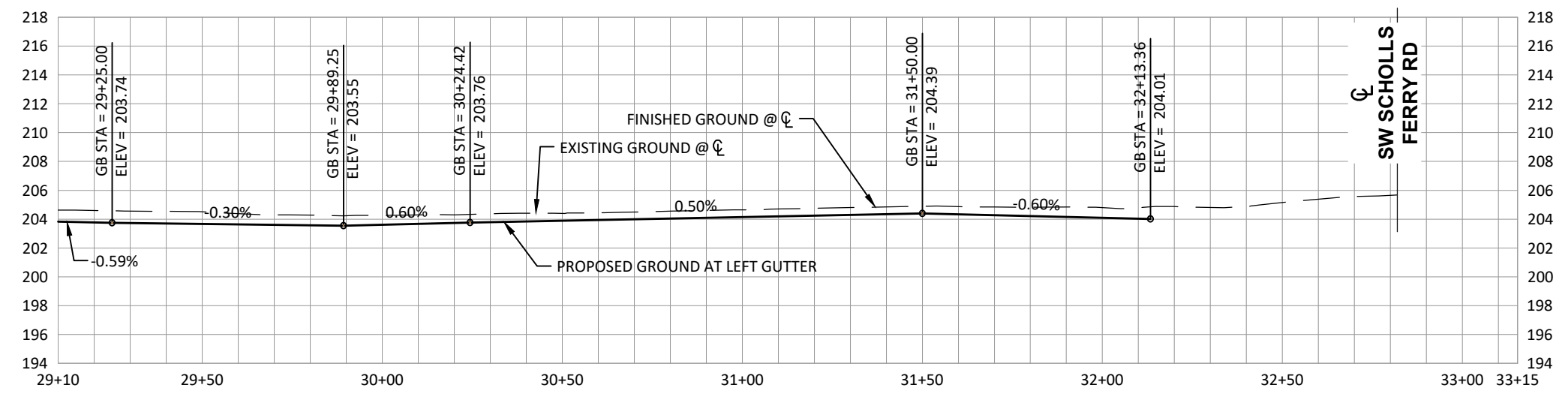
PROJECT NUMBER
100666
SHEET NO.
47 OF 84
SHEET TITLE
7



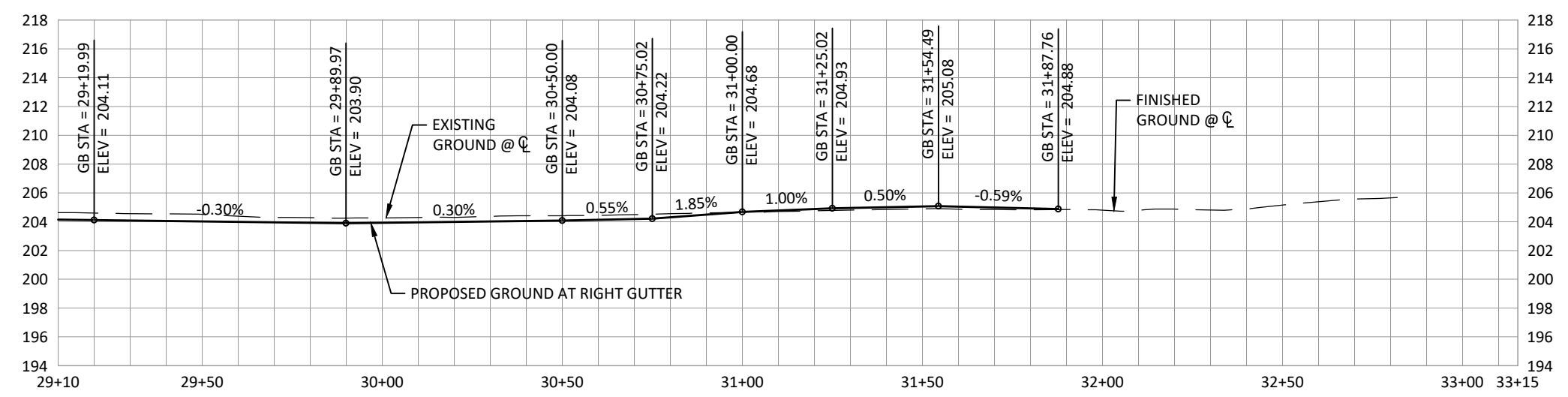
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Department of Land Use & Transportation Engineering



SW DENNEY RD - LEFT PROFILE
 SCALE: 1" = 40' (HORZ.)
 1" = 10' (VERT.)



SW DENNEY RD - RIGHT PROFILE
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 1" = 10' (VERT.)

NO.	REVISION
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SW DENNEY ROAD
 SW 105TH AVE TO SW SCHOLLS FERRY ROAD
 WASHINGTON COUNTY

ROADWAY PROFILES

PROJECT NUMBER
100666

SHEET NO.
48 OF **84**
 SHEET TITLE
7A